

Date of issue: 21st May, 2015

MEETING

PLANNING COMMITTEE

(Councillors Dar (Chair), M Holledge, Ajaib, Bains, Chaudhry, Davis, Plenty, Smith and Swindlehurst)

DATE AND TIME:

MONDAY, 1ST JUNE, 2015 AT 6.30PM

VENUE:

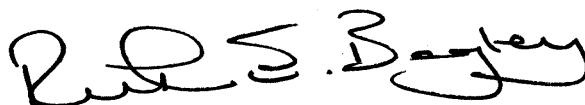
FLEXI HALL, THE CENTRE, FARNHAM ROAD, SLOUGH, SL1 4UT

**DEMOCRATIC SERVICES
OFFICER:
(for all enquiries)**

TERESA CLARK
01753 875018

NOTICE OF MEETING

You are requested to attend the above Meeting at the time and date indicated to deal with the business set out in the following agenda.



RUTH BAGLEY
Chief Executive

AGENDA

PART 1

**AGENDA
ITEM**

REPORT TITLE

PAGE

WARD

1. Apologies for Absence

CONSTITUTIONAL MATTERS

2. Declarations of Interest

All Members who believe they have a Disclosable Pecuniary or other Pecuniary or non pecuniary Interest in any matter to be considered at the meeting must declare that interest and,



| <u>AGENDA ITEM</u> | <u>REPORT TITLE</u> | <u>PAGE</u> | <u>WARD</u> |
|------------------------|---|-------------|------------------------|
| | <p><i>having regard to the circumstances described in Section 3 paragraphs 3.25 – 3.27 of the Councillors' Code of Conduct, leave the meeting while the matter is discussed, save for exercising any right to speak in accordance with Paragraph 3.28 of the Code.</i></p> <p><i>The Chair will ask Members to confirm that they do not have a declarable interest.</i></p> <p><i>All Members making a declaration will be required to complete a Declaration of Interests at Meetings form detailing the nature of their interest.</i></p> | | |
| 3. | Guidance on Predetermination/Predisposition - To Note | 1 - 2 | |
| 4. | Minutes of the Last Meeting held on 29th April, 2015 | 3 - 6 | |
| 5. | Human Rights Act Statement - To Note | 7 - 8 | |
| | PLANNING APPLICATIONS | | |
| 6. | P/00176/033 - 392 Bath Road, Slough, SL1 6JA Officer Recommendation: Delegate to Planning Manager | 9 - 30 | Haymill and Lynch Hill |
| 7. | P/16111/000 - Land Rear Of, 102-104 Farnham Road, Slough, Berkshire, SL1 4YS Officer Recommendation: Delegate to Planning Manager | 31 - 40 | Farnham |
| 8. | P/00322/019 - Greenwatt Way, Slough, Berkshire, SL1 3SJ Officer Recommendation: Delegate to Planning Manager | 41 - 50 | Chalvey |
| 9. | P/16122/000 - Driving Standards Agency, Driving Test Centre, Grays Place, Slough, SL2 5AF Officer Recommendation: Delegate to Planning Manager | 51 - 60 | Central |
| 10. | P/16138/000 - 9-11 David Road, Poyle Trading Estate, Colnbrook, Slough, Berkshire, SL3 0DB Officer Recommendation: Delegate to Planning Manager | 61 - 72 | Colnbrook with Poyle |



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| 11. | P/14486/001 - Unit 1 Prescott Road, Colnbrook, Slough, SL3 0AE | 73 - 92 | Colnbrook with Poyle |

Officer Recommendation: Delegate to Planning Manager

MATTERS FOR INFORMATION

| | | |
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| 13. | Members Attendance Record | 95 - 96 |
| 14. | Date of Next Meeting | |

1st July, 2015

Press and Public

You are welcome to attend this meeting which is open to the press and public, as an observer. You will however be asked to leave before the Committee considers any items in the Part II agenda. Please contact the Democratic Services Officer shown above for further details.

The Council allows the filming, recording and photographing at its meetings that are open to the public. Anyone proposing to film, record or take photographs of a meeting is requested to advise the Democratic Services Officer before the start of the meeting. Filming or recording must be overt and persons filming should not move around the meeting room whilst filming nor should they obstruct proceedings or the public from viewing the meeting. The use of flash photography, additional lighting or any non hand held devices, including tripods, will not be allowed unless this has been discussed with the Democratic Services Officer.

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PREDETERMINATION/PREDISPOSITION - GUIDANCE

The Council often has to make controversial decisions that affect people adversely and this can place individual members in a difficult position. They are expected to represent the interests of their constituents and political party and have strong views but it is also a well established legal principle that members who make these decisions must not be biased nor must they have pre-determined the outcome of the decision. This is especially so in “quasi judicial” decisions in planning and licensing committees. This Note seeks to provide guidance on what is legally permissible and when members may participate in decisions. It should be read alongside the Code of Conduct.

Predisposition

Predisposition is lawful. Members may have strong views on a proposed decision, and may have expressed those views in public, and still participate in a decision. This will include political views and manifesto commitments. The key issue is that the member ensures that their predisposition does not prevent them from consideration of all the other factors that are relevant to a decision, such as committee reports, supporting documents and the views of objectors. In other words, the member retains an “open mind”.

Section 25 of the Localism Act 2011 confirms this position by providing that a decision will not be unlawful because of an allegation of bias or pre-determination “just because” a member has done anything that would indicate what view they may take in relation to a matter relevant to a decision. However, if a member has done something more than indicate a view on a decision, this may be unlawful bias or predetermination so it is important that advice is sought where this may be the case.

Pre-determination / Bias

Pre-determination and bias are unlawful and can make a decision unlawful. Predetermination means having a “closed mind”. In other words, a member has made his/her mind up on a decision before considering or hearing all the relevant evidence. Bias can also arise from a member’s relationships or interests, as well as their state of mind. The Code of Conduct’s requirement to declare interests and withdraw from meetings prevents most obvious forms of bias, e.g. not deciding your own planning application. However, members may also consider that a “non-pecuniary interest” under the Code also gives rise to a risk of what is called apparent bias. The legal test is: “whether the fair-minded and informed observer, having considered the facts, would conclude that there was a real possibility that the Committee was biased”. A fair minded observer takes an objective and balanced view of the situation but Members who think that they have a relationship or interest that may raise a possibility of bias, should seek advice.

This is a complex area and this note should be read as general guidance only. Members who need advice on individual decisions, should contact the Monitoring Officer.

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Planning Committee – Meeting held on Wednesday, 29th April, 2015.

Present:- Councillors Dar (Chair), Ajaib (Vice-Chair) (from 7.17 pm), Bains, M Holledge, Plenty, Sidhu, Smith (until 8.48 pm) and Swindlehurst (from 6.45 pm).

Also present under Rule 30:- Councillors Coad and Brooker

Apologies for Absence:- Councillor Rasib

PART I

116. Apologies for Absence

An apology was received from Councillor Rasib.

117. Declarations of Interest

Councillor Holledge declared an interest in respect of Agenda item 9 – P/02631/018 - The Langley Academy, Langley Road, Slough, in that it was situated in his Ward. He advised that he had an open mind and would debate and vote on the item.

Councillor Sidhu declared an interest in respect of Agenda item 8 - P2684/010 - Former BT Depot & 297 Langley Road, Slough, in that it was situated in his Ward. He advised that he had an open mind and would debate and vote on the item.

118. Guidance on Predetermination/Predisposition

Members confirmed that they had read and understood the guidance on predetermination and predisposition.

119. Minutes of the Last Meeting held on 1st April, 2015

Resolved - That the minutes of the meeting held on 1st April, 2015 be approved as a correct record.

120. Human Rights Act Statement

The Human Rights Act statement was noted.

121. Planning Applications

Details were tabled in the amendment sheet of alterations and amendments received since the agenda was circulated. The Committee adjourned for ten minutes to allow Members the opportunity to read the amendment sheet.

Planning Committee - 29.04.15

Oral representations were made to the Committee by an objector and agent under the Public Participation Scheme and local members prior to the planning applications being considered by the Committee as follows:-

Application- P/02684/010 - Former BT Depot & 297 Langley Road, Slough: a Ward Member (Councillor Coad) addressed the Committee.

Application- P/02631/018 - The Langley Academy, Langley Road, Slough: a Registered Objector, the Applicant's Agent, and a Ward Member (Councillor Brooker) addressed the Committee.

The Chair varied the order of agenda so that the item where Objectors were in attendance was taken first and the item where a Ward Member wished to address the Committee was taken second.

Resolved – That the decisions be taken in respect of the planning applications as set out in the minutes below, subject to the information, including conditions and informatives set out in the report of the Head of Planning Policy and Projects and the amendments sheet tabled at the meeting and subject to any further amendments and conditions agreed by the Committee.

122. P/01412/012 - Sapphire Court 274-276, High Street, Slough, Berkshire, SL1 1NB

| Application | Decision |
|--|---|
| Erection of additional floor and five storey rear extension together with elevational changes and change of use of upper floors to residential accommodation comprising a total of 12 flats (9no x 1 bed and 3no x 2 bed). Also ground floor rear extension. | Delegated to the Planning Manager for minor design changes, completion of a section 106 agreement, finalising conditions and final determination. |

123. P/08145/005 - Salisbury House, 300-310 High Street, Slough, SL1 1NB

| Application | Decision |
|--|---|
| Construction of an additional floor and change of use of upper floors to residential accommodation comprising a total of 30 flats (25no x 1 bed and 5no x 2 bed), fenestration changes and rear fire escape. | Delegated to the Planning Manager for resolution of outstanding highway and transport issues, minor design changes, completion of a Section 106 agreement, finalising conditions and final determination. |

Planning Committee - 29.04.15

124. P/02684/010 - Former BT Depot & 297 Langley Road, Slough

| Application | Decision |
|---|---|
| Residential development comprising 129 dwellings (20 no. 1 bedroom flats, 34 no. 2-bedroom flats, 17no. 2-bed houses, 47 no. 3-bedroom houses & 11no.4-bedroom houses), construction of new roundabout on Langley Road at the entrance to the site, and associated open space, access roads, car parking and landscaping works (outline application). | Delegated to the Planning Manager to review amendments to ensure the majority of highway is designed to an adoptable standard and that Officers negotiate to try to achieve the adoption of highways. That the application be referred back to the Committee if not resolved. |

125. P/02631/018 - The Langley Academy, Langley Road, Slough

| Application | Decision |
|--|--|
| Erection of two storey primary school with nursery and associated access, car and cycle parking, play and amenity space. | Deferred for further consideration by the Planning Officer of the Cockett Road entrance to the site, to consider highway implications, to examine the option of a 4 th access at the southern boundary of the site, and to be referred to the next Committee meeting. |

126. Planning Appeal Decisions

Resolved - That details of recent Planning Appeal decisions be noted.

127. Members Attendance Record

Resolved - That the Members Attendance Record for 2014/15 be noted.

128. Date of Next Meeting

The date of the next meeting was confirmed as Monday 1st June, 2015.

Chair

(Note: The Meeting opened at 6.30pm and closed at 9.10pm)

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The Human Rights Act 1998 was brought into force in this country on 2nd October 2000, and it will now, subject to certain expectations, be directly unlawful for a public authority to act in a way which is incompatible with a Convention Right. In particular Article 8 (Respect for Private and Family Life) and Article 1 of Protocol 1 (Peaceful Enjoyment of Property) apply to planning decisions. When a planning decision is to be made, however, there is further provision that a public authority must take into account the public interest. In the vast majority of cases existing planning law has for many years demanded a balancing exercise between private rights and public interest, and therefore much of this authority's decision making will continue to take into account this balance.

The Human Rights Act 1998 will not be referred to in the Officers Report for individual applications beyond this general statement, unless there are exceptional circumstances which demand more careful and sensitive consideration of Human Rights issues.

Please note the Ordnance Survey Maps for each of the planning applications are not to scale and measurements should not be taken from them. They are provided to show the location of the application sites.

| | |
|------------|---|
| CLU / CLUD | Certificate of Lawful Use / Development |
| GOSE | Government Office for the South East |
| HPSP | Head of Planning and Strategic Policy |
| HPPP | Head of Planning Policy & Projects |
| S106 | Section 106 Planning Legal Agreement |
| SPZ | Simplified Planning Zone |
| TPO | Tree Preservation Order |
| LPA | Local Planning Authority |

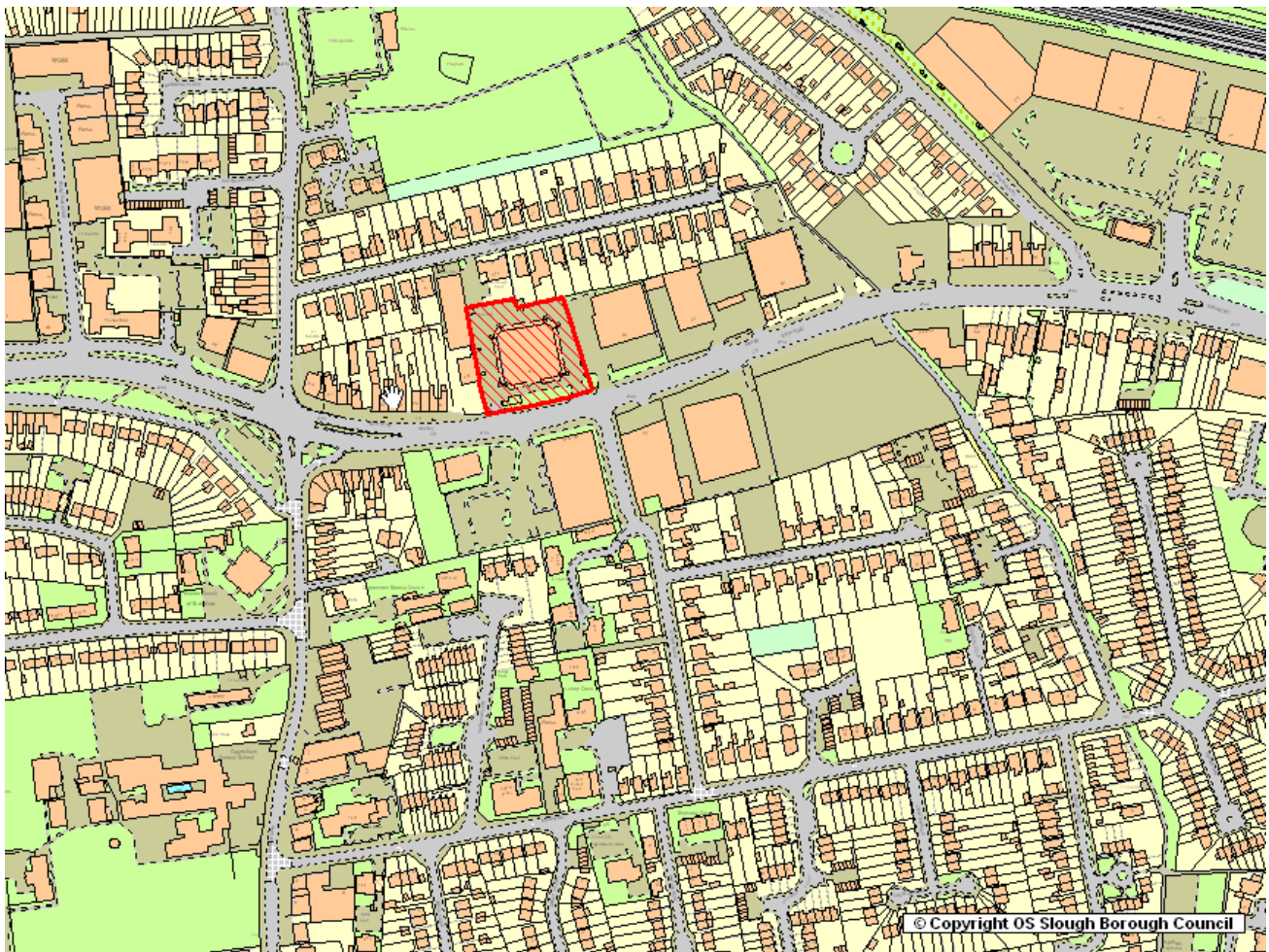
| | USE CLASSES – Principal uses |
|---------|-------------------------------------|
| A1 | Retail Shop |
| A2 | Financial & Professional Services |
| A3 | Restaurants & Cafes |
| A4 | Drinking Establishments |
| A5 | Hot Food Takeaways |
| B1 (a) | Offices |
| B1 (b) | Research & Development |
| B1 (c) | Light Industrial |
| B2 | General Industrial |
| B8 | Warehouse, Storage & Distribution |
| C1 | Hotel, Guest House |
| C2 | Residential Institutions |
| C2(a) | Secure Residential Institutions |
| C3 | Dwellinghouse |
| C4 | Houses in Multiple Occupation |
| D1 | Non Residential Institutions |
| D2 | Assembly & Leisure |

| | OFFICER ABBREVIATIONS |
|----|------------------------------|
| WM | Wesley McCarthy |
| PS | Paul Stimpson |
| CS | Chris Smyth |
| JD | Jonathan Dymond |
| HA | Howard Albertini |
| IH | Ian Hann |
| NR | Neetal Rajput |
| SB | Sharon Belcher |
| AM | Ann Mead |
| FI | Fariba Ismat |

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| | | | |
|--------------------|--|---------------|-----------------------------------|
| Registration Date: | 22-Jan-2015 | Applic. No: | P/00176/033 |
| Officer: | Mr. J. Dymond | Ward: | Haymill |
| | | Applic type: | Major |
| | | 13 week date: | 23rd April 2015 |
| Applicant: | Mr. Charles Slaughter, Trading as Grafise Limited | | |
| Agent: | Ernest Ansah, MEDA The Granary, Church Lane, Steventon, Oxfordshire, OX13 6SW | | |
| Location: | 392, Bath Road, Slough, SL1 6JA | | |
| Proposal: | DEMOLITION OF EXISTING BUILDING AND CHANGE OF USE OF SITE, AND ERECTION OF A NEW AUTOMOTIVE RETAIL DEALERSHIP, TO INCLUDE A NEW CAR SHOWROOM, WORKSHOP, MOT, VALETING, VEHICLE DISPLAY AND PARKING FACILITIES. | | |

Recommendation: Delegate to Planning Manager



SUPPLEMENTARY REPORT TO PLANNING COMMITTEE

1.0 Background

1.1 At the meeting of the Planning Committee on 1st April 2015, the Committee resolved to delegate the decision to the Planning Manager to negotiate relocation of rear door to east or west boundary. If the relocation of the door was not possible, the application was to be brought back to Planning Committee for further discussion.

1.2 A copy of the officer's report to the Committee on 1st April 2015 (Appendix A) and associated amendments (Appendix B) are attached for information purposes.

2.0 Issues Regarding Relocation Of Rear Door

2.1 Following the Committee meeting on 1st April 2015, the applicant has investigated the relocation of rear door to east or west boundary.

2.2 The applicant has responded confirming that they have explored with their architect the possibility of acceding to this request, whilst at the same time meeting their operational needs, and also those of the manufacturer that they represent.

2.3 An Operational Statement explaining the negative impact of moving the workshop doors has been received, along with a copy of a document explaining the principles upon which the workshop will be run by the operator.

2.4 The applicant considers that the best solution that would achieve the desired change would result in several issues that would be detrimental to the operation of the site and also to the viability of the scheme.

2.5 A copy of the applicant's Operational Statement regarding the relocation of the rear door is attached at Appendix C.

2.6 The applicant has also confirmed the following:

- There is only one MOT bay in a side-by-side configuration within the facility;
- This bay is located 5m from the external wall on the north boundary;
- The noise from MOT testing would not be continuous, only occurring for short periods of time during which the workshop doors would be kept shut.

2.7 The applicant is also agreeable to submitting a Noise Management Plan and it has been recommended that this is secured by condition. The applicant has confirmed that the workshop doors would be kept shut during MOT testing and this could also be secured by condition.

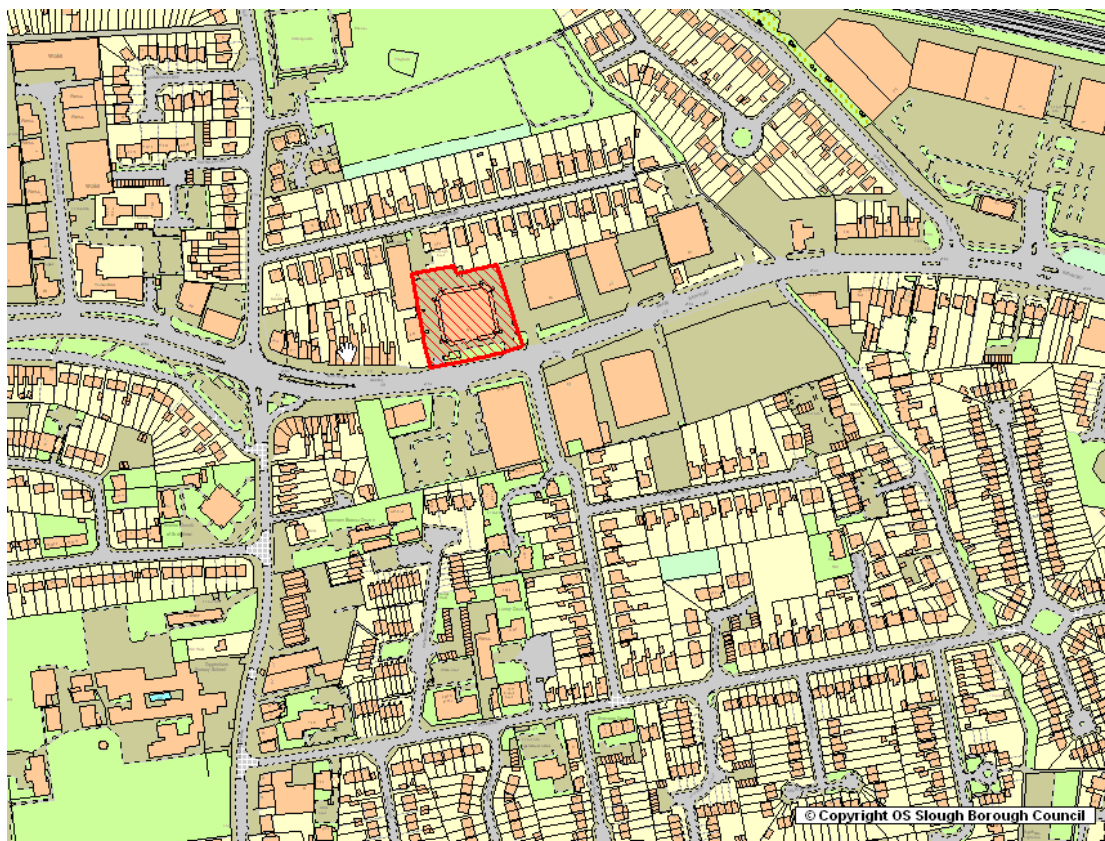
3.0 RECOMMENDATION

3.1 Delegate a decision to the Planning Manager to agree any minor amendments to the planning application, draft conditions and Section 106 planning obligation matters.

Appendix A

| | | | |
|---------------------------|--|----------------------|-----------------------------|
| Registration Date: | 22-Jan-2015 | Applic. No: | P/00176/033 |
| Officer: | Mr. J. Dymond | Ward: | Haymill and Lynch Hill |
| | | Applic type: | Major |
| | | 13 week date: | 23 rd April 2015 |
| Applicant: | Mr. Charles Slaughter, Trading as Grafise Limited | | |
| Agent: | Ernest Ansah, MEDA The Granary, Church Lane, Steventon, Oxfordshire, OX13 6SW | | |
| Location: | 392 Bath Road, Slough, SL1 6JA | | |
| Proposal: | DEMOLITION OF EXISTING BUILDING AND CHANGE OF USE OF SITE, AND ERECTION OF A NEW AUTOMOTIVE RETAIL DEALERSHIP, TO INCLUDE A NEW CAR SHOWROOM, WORKSHOP, MOT, VALETING, VEHICLE DISPLAY AND PARKING FACILITIES. | | |

Recommendation: Delegate to Planning Manager



1.0 **SUMMARY OF RECOMMENDATION**

- 1.1 This application has been referred to the Planning Committee for consideration as the application is for a major development.
- 1.2 Having considered the relevant policies set out below, the comments and representations received, and all other relevant material considerations, it is recommended to delegate a decision to the Planning Manager for Section 106.

PART A: BACKGROUND

2.0 **Proposal**

- 2.1 The proposal is for the demolition of existing building and change of use of site, and erection of a new automotive retail dealership, to include a new car showroom, workshop, MOT, valeting, vehicle display and parking facilities.

3.0 **Application Site**

- 3.1 The site is 0.4 of a hectare in area and is broadly square in shape. The existing building on the site has a floor area of 2,665 square metres and is two storeys in height. The building fronts Bath Road. The nature of the surrounding uses when viewed from Bath Road is generally commercial in nature. There are residential properties beyond the rear boundary of the site.
- 3.2 Marlborough Court is situated beyond the north western corner of the site and the properties of Iona Crescent (nos. 26-30) are situated to the rear and to the north east. Iona Crescent is a cul-de-sac which is accessed off of Station Road to the west.
- 3.3 To the east of the site is 380 Bath Road. This building is currently occupied by Halfords, a retailer selling cycle and motoring products. To the west of the site is 396 Bath Road. This unit is used for the sale of carpets. On the opposite side of the road to the south is 383-389 Bath Road. This building is in use for retail purposes, and the unit adjacent to Bath Road is a bed superstore.
- 3.4 The site has two access points onto Bath Road. There are car parking spaces surrounding the building and abutting the northern, eastern and western boundaries.
- 3.5 The boundary treatment is timber fencing to the rear and sides. There is a wall along the front boundary.

4.0 **Site History**

- 4.1 Recent applications relating to the site are as follows:

P/00176/034 ADVERTISEMENT CONSENT FOR DISPLAY OF SIGNAGE
COMPRISING 1NO. FREE-STANDING ILLUMINATED PYLON, 1NO.

WALL-MOUNTED ILLUMINATED PANEL SIGN, 3NO. FREE-STANDING FLAGS, 7NO. POST-MOUNTED NON-ILLUMINATED PARKING SIGNS, 1NO. FREE-STANDING INTERNALLY ILLUMINATED DIRECTIONAL SIGN, 1NO. FREE-STANDING COMMUNICATIONS PANEL, 3NO. FREE-STANDING INFORMATION TOTEMS, 2NO. WALL-MOUNTED APPLIED VINYL INFORMATION SIGNS.

Application under consideration

P/00176/032 CHANGE OF USE FROM OFFICES (CLASS B1A) TO RESTURANT (CLASS A3), WITH SEATING AT GROUND FLOOR ONLY AND STORAGE/ FOOD PREPARATION AT FIRST FLOOR, INSTALLATION OF NEW SERVICE ENTRANCE, MINOR ENTENAL WORKS TO PROVIDE CYCLE PARKING AND CHANGES TO PARKING LAYOUT.

Withdrawn (Treated As)

P/00176/031 CHANGE OF USE OF EXISTING BUILDING FROM CLASS B1 (BUSINESS) TO PART SUI GENERIS (BANQUETING) AT FIRST FLOOR AND PART CLASS A3 (RESTAURANT) AT GROUND FLOOR. MINOR EXTERNAL ALTERATIONS TO IMPROVE STAIR ESCAPE WIDTHS FROM FIRST FLOOR

Withdrawn (Treated As)

P/00176/030 VARIATION OF CONDITION 3 AND 4 OF PLANNING PERMISSION P/00176/021 TO ALLOW UNRESTRICTED USE WITHIN THE B1 USE CLASS

Approved with Conditions; Informatives 21-Feb-2007

5.0 **Neighbour Notification**

5.1 9, Stowe Road, Slough, SL1 5QE, Halfords Ltd, 380, Bath Road, Slough, SL1 6JA, H S S Hire Group Plc, 375, Bath Road, Slough, SL1 5QA, 15, Stowe Road, Slough, SL1 5QE, 23, Iona Crescent, Slough, SL1 6JH, 29, Burnham Lane, Slough, SL1 6LH, 14, Masons Road, Slough, SL1 5QJ, 5, Iona Crescent, Slough, SL1 6JH, Flat 3, Compton Court, Brook Crescent, Slough, SL1 6LL, 25, Stanhope Road, Slough, SL1 6JR, 17, Stowe Road, Slough, SL1 5QE, 43, Iona Crescent, Slough, SL1 6JH, Wyeth Research, 392, Bath Road, Slough, SL1 6JA, 4, Stowe Road, Slough, SL1 5QF, Himiliya Carpet, 396, Bath Road, Slough, SL1 6JA, 22, Stowe Road, Slough, SL1 5QF, 35, Iona Crescent, Slough, SL1 6JH, 30, Iona Crescent, Slough, SL1 6JH, 24, Stowe Road, Slough, SL1 5QF, 7, Iona Crescent, Slough, SL1 6JH, Jag Dev Autos, 398, Bath Road, Slough, SL1 6JA, 383-389, Bath Road, Slough, SL1 5QA, 23, Stanhope Road, Slough, SL1 6JR, 28, Iona Crescent, Slough, SL1 6JH, 42, Iona Crescent, Slough, SL1 6JH, 19, Iona Crescent, Slough, SL1 6JH, 17, Burnham Lane, Slough, SL1 6LH, 33, Iona Crescent, Slough, SL1 6JH, 34, Iona Crescent, Slough, SL1 6JH, 21, Suffolk Close, Slough, SL1 6JN, 16a, Stowe Road, Slough, SL1 5QF,

39, Stanhope Road, Slough, SL1 6JR, 26, Iona Crescent, Slough, SL1 6JH, 16, Balmoral Close, Slough, SL1 6JP, 25, Iona Crescent, Slough, SL1 6JH, Flat 4, 24, Iona Crescent, Slough, SL1 6JH, Flat 5, 24, Iona Crescent, Slough, SL1 6JH, Flat 6, 24, Iona Crescent, Slough, SL1 6JH, Flat 7, 24, Iona Crescent, Slough, SL1 6JH, Flat 1, 24, Iona Crescent, Slough, SL1 6JH, Flat 2, 24, Iona Crescent, Slough, SL1 6JH, Flat 3, 24, Iona Crescent, Slough, SL1 6JH, Flat 8, 24, Iona Crescent, Slough, SL1 6JH, Flat 9, 24, Iona Crescent, Slough, SL1 6JH, 11, Masons Road, Slough, SL1 5QJ, Flat 9, Marlborough Court, Iona Crescent, Slough, SL1 6JH, Flat 6, Marlborough Court, Iona Crescent, Slough, SL1 6JH, 17, Jupiter Court, Slough, SL1 5QG

5.2 One objection has been received from a resident of Marlborough Court, Iona Crescent, and the concerns raised in summary are as follows:

5.3 – Proposal has not made adequate access arrangements – the dealership may exacerbate the existing traffic levels and give rise to a build-up of traffic;

Response: The assessment of trips shows that the development will lead to a reduction in traffic flows during the peak periods compared to office trips.

– Loss of privacy and glare from signage;

Response: The rear elevation of the building is proposed to be solid with no window openings. It is therefore considered that the proposal would not give rise to any potential overlooking issues to the rear. A condition is to be recommended to state that no windows should be formed in this elevation. The signage is to be sited to the front of the building facing Bath Road and it is not considered that glare from signage would have an adverse impact on the privacy of occupiers to the rear.

– Noise disturbance as a result of motor workshop, deliveries and associated noise, valet service and machinery;

Response: The Council's Environmental Protection section has been consulted. The applicant has submitted a noise assessment and this has been considered. Conditions have been recommended to address potential noise issues and a noise management plan will also be required to address operational matters.

– Car fumes;

Response: It is not considered that the proposal would give rise to unacceptable impacts on neighbour amenity as a result of fumes. In this regard it is noted that there is an existing car park around the building at present which has in the past, and could indeed potentially today be used as a car park.

– Light pollution;

Response: The glazed showroom element of the proposal is to be located to the front of the building facing Bath Road. A lighting scheme will be required by condition.

- Effect on trees;

Response: The Council's Tree Officer has been consulted and has not raised an objection on tree grounds. It is however considered that it would be desirable to have some further vegetation on the site where possible and it is recommended that a detailed landscape plan is required by condition to mitigate the loss of the existing vegetation.

- Inadequate landscaping/means of enclosure;

Response: The Council's Tree Officer has been consulted and has recommended that a detailed landscape plan is required by condition to mitigate the loss of the existing vegetation. A condition regarding boundary treatment will also be recommended.

- Design of means the building and signage appearance will have a negative effect on the surrounding area;

Response: The design and appearance of the building is considered to be acceptable and inkeeping with the surrounding area. Signage is to be assessed under a separate advertisement consent application.

- Impact of the proposed dealership works against relevant planning policies.

Response: In summary, the proposal is considered to be acceptable and in accordance with the aims of relevant planning policies.

6.0 **Consultation**

6.1 **Transport and Highways**

No objections subject to conditions and Section 106 obligations. Comments as follows in summary:

- The assessment of trips shows that the development will lead to a reduction in traffic flows during the peak periods compared to office trips;
- Access to the site will be managed by signage and road markings;
- Both existing access points will be improved to facilitate large vehicles turning left in and left out of the site onto Bath Road;
- The transporter that will deliver the cars that are to be sold to the public will visit the site only once every one to two weeks. Cars to be used in the showroom will be delivered at a lower frequency;
- The deliveries will be scheduled to allow time for customer parking to be managed effectively;
- Visibility from the site egress should meet 2.4m x 90m and this should be secured by a planning condition;
- The proposed number of spaces is considered appropriate for the sui generis use;

- A framework travel plan has been submitted with the transport statement – this was not compulsory but the efforts are welcomed.

6.2 **Environmental Protection**

No objections subject to conditions.

6.3 **Tree Officer**

No objections subject to a condition. Comments as follows in summary:

- The application proposes to remove the vegetation on the Bath Road; this vegetation would soon be lost to road widening in any case;
- It is considered desirable to have some vegetation on the Bath Road boundary to soften the view of the proposed development and it is considered that this can be achieved;
- There is enough space for vegetation on the eastern boundary to be retained or replaced;
- It would be desirable to have some further vegetation on the site where possible;
- It is recommended that a detailed landscape plan is required by condition to mitigate the loss of the existing vegetation.

PART B: PLANNING APPRAISAL

7.0 **Policy Background**

7.1 The following policies are considered most relevant to the assessment of this application:

The National Planning Policy Framework and the Planning Practice Guidance

The Slough Local Development Framework, Core Strategy 2006 – 2026, Development Plan Document

Core Policy 1 – Spatial Strategy

Core Policy 5 – Employment

Core Policy 7 – Transport

Core Policy 8 – Sustainability and the Environment

Core Policy 12 – Community Safety

The Local Plan for Slough, Adopted March 2004

Policy EN1 – Standard of Design

Policy EN5 – Design and Crime Prevention

Policy T2 – Parking Restraint

Policy T8 – Cycling Network and Facilities

Policy EMP2 – Criteria for Business Developments

Policy EMP12 – Remaining Existing Business Areas

Slough Local Development Framework Site Allocations Development Plan Document Selected Location for Comprehensive Regeneration – SKL1

Composite Local Plan – Slough Local Development Plan and the NPPF - PAS Self Assessment Checklist

Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires that applications for planning permission are determined in accordance with the development plan unless material considerations indicate otherwise. Annex 1 to the National Planning Policy Framework advises that due weight should be given to relevant policies in existing plans according to their degree of consistency with the Framework (the closer the policies in the plan to the policies in the Framework, the greater the weight that may be given).

The Local Planning Authority has published a self assessment of the Consistency of the Slough Local Development Plan with the National Planning Policy Framework using the PAS NPPF Checklist.

The detailed Self Assessment undertaken identifies that the above policies are generally in conformity with the National Planning Policy Framework. The policies that form the Slough Local Development Plan are to be applied in conjunction with a statement of intent with regard to the presumption in favour of sustainable development.

It was agreed at Planning Committee in October 2012 that it was not necessary to carry out a full scale review of Slough's Development Plan at present, and that instead the parts of the current adopted Development Plan or Slough should all be republished in a single 'Composite Development Plan' for Slough. The Planning Committee endorsed the use of this Composite Local Plan for Slough in July 2013.

7.2 The main planning issues relevant to the assessment of this application are considered to be as follows:

- 1) Principle of development;
- 2) Design and Impact on the street scene;
- 3) Potential impact on neighbouring properties;
- 4) Parking and highway safety;
- 5) Heads of terms;
- 6) Other issues.

8.0 **Principle of Development**

- 8.1 The building fronts Bath Road and the nature of the surrounding uses when viewed from Bath Road is generally commercial in nature. There are residential properties beyond the rear boundary of the site.
- 8.2 The building was formerly in use for B1(a) office purposes however it is understood to be currently vacant.
- 8.3 The site is located within an existing business area as illustrated on the Core Strategy Key Diagram.
- 8.4 The site is identified as an Area of Major Change in the Core Strategy. The western

end of the A4 Bath Road is identified in paragraph 7.98 as having the potential to accommodate some alternative uses or mixed use developments.

- 8.5 The site therefore forms part of site allocation SKL1 in the Site Allocations Development Plan Document. The allocation allows for the loss of the existing business area and seeks to achieve the comprehensive regeneration of the Trade Sales site which is to the east of the application site, and surrounding sites.
- 8.6 There is considered to be no objection to the loss of the office use. It is understood that this building has been vacant for some time. The principle of the proposed use is considered to be acceptable as the proposal would constitute an employment generating use and would be inkeeping with the commercial nature of this Bath Road frontage location.
- 8.7 The submitted application form states that the proposal would provide 30 full time jobs and the proposal would therefore provide employment opportunities and would accord with Core Policy 5 of the Core Strategy. Furthermore, the proposal would accord with the aims of the identified Area of Major Change.
- 8.8 The proposal would be consistent with its location and would comply with Core Policies 1, 5 and 6 of the Core Strategy and Policy EN1 of the Adopted Local Plan for Slough.

9.0 **Design and Impact on the Street Scene**

- 9.1 The thrust of Policy EN1 of The Adopted Local Plan for Slough and Core Policy 8 of the Core Strategy is that the design of proposed development should be of a high standard of design and should reflect the character and appearance of the surrounding area.
- 9.2 The proposed building would have a height of 7.4 metres and would contain the following:
- Ground Floor; Gross external area of 1130 sq m, zoned into three sections, to include internal areas of;
 - Showroom display, customer lounge area and front-of-house environment – 350 sq m
 - Offices, customer facilities and back-of-house environment – 85 sq m
 - Parts Stores, and Workshop facilities including 7 workbays, 2 Mot Bays and valeting facilities – 640 sq m
 - First Floor; Gross external area of 240 sq m (2, 583 sq ft) will house offices, staff and Mechanics' facilities, taking up an internal floor space of 230 sq m.
- 9.3 The proposed building would be broadly rectangular in shape and would be sited in a similar position on the site to the existing building.
- 9.4 Proposed materials are considered to be acceptable and inkeeping with the surrounding area. An application for advertisement consent is also currently under consideration.

- 9.5 The proposal has been subject to a BREEAM pre-assessment in which a “Very Good’ rating has been identified.
- 9.6 It is considered that the proposal would be acceptable in design terms and compliant with Core Policy 8 of the Core Strategy and Policy EN1 of The Adopted Local Plan for Slough.
- 10.0 **Potential Impact on Neighbouring Properties**
- 10.1 Concerns have been raised in the representation received regarding the potential impact of the proposal on the amenity of nearby neighbouring residents. There are neighbouring properties beyond the northern boundary of the site and rear gardens back onto the rear of the existing car park.
- 10.2 Core Policy 8 of the Core Strategy states that development will respect its location and surroundings, and respect the amenities of adjoining occupiers.
- 10.3 Policy EMP2 of the Adopted Local Plan states that proposals for business developments will only be permitted if there is no significant loss of amenities for the neighbouring land uses as a result of noise, the level of activity, and overlooking.
- 10.4 The proposed building is not considered to give rise to a loss of light, overshadowing or overlooking to neighbouring properties.
- 10.5 **With regard to hours of use, the applicant has stated that these would be as follows for each element of the proposal:**

| | Monday to Friday | | Saturday | | Sunday and Bank Holidays | |
|----------------|------------------|----------|------------|----------|--------------------------|----------|
| | Start Time | End Time | Start Time | End Time | Start Time | End Time |
| A1 (Showroom) | 08:00 | 18:00 | 08:00 | 18:00 | 10:00 | 16:00 |
| B1A (Office) | 08:00 | 18:00 | 08:00 | 18:00 | 10:00 | 16:00 |
| B1C (Workshop) | 08:00 | 18:00 | 08:00 | 18:00 | Closed | Closed |

- 10.6 It is considered that these hours of use would be consistent with other similar commercial uses in the surrounding area and would be acceptable in amenity terms.
- 10.7 Turning to other neighbour impact issues, the Council’s Environmental Protection section have been consulted. The applicant has submitted a noise assessment and this has been considered. Conditions have been recommended to address potential noise issues. The cladding to be used would also have sound reduction properties.
- 10.8 With regard to operational noise, the noise assessment recommends that must be

addressed by suitable management control, and a statement to that effect is to be completed by the users. A condition is recommended regarding this.

- 10.9 With regard to hours of deliveries, no deliveries will take place outside of recommended working hours, with no deliveries to be made on Saturday afternoons after 1pm, Sundays and bank holidays.
- 10.10 The workshop would be accessed from the rear however most customer vehicular movements would take place within the forecourt to the front.
- 10.11 On this basis and subject to appropriate planning conditions, it is considered that the proposed development would not give rise to unacceptable neighbour impacts and would be acceptable in planning terms. It is therefore considered that the proposal would be acceptable in terms of amenity and compliant with Core Policy 8 of the Core Strategy and Policy EMP2 of the Adopted Local Plan for Slough.

11.0 **Parking and Highway Safety**

- 11.1 Core Policy 7 of the Core Strategy sets out the Planning Authority's approach to the consideration of transport matters. The thrust of this policy is to ensure that new development is sustainable and is located in the most accessible locations, thereby reducing the need to travel.
- 11.2 Policy T2 of The Adopted Local Plan for Slough seeks to restrain levels of parking in order to reduce the reliance on the private car through the imposition of parking standards.
- 11.3 The Bath road widening line affects the front of the site, and this has been incorporated into the scheme. Tracking drawings for transporters also indicates that all servicing can take place on site. Circulation on the site is proposed as a one-way system with access via the west, and the exit to the east. The existing east exit has been relocated to improve visibility and to accommodate transporter manoeuvres out of the site onto the Bath road.
- 11.4 The Council's Transport consultant has been consulted. No objections have been raised subject to conditions and Section 106 obligations. These matters are set out in detail in the section below.
- 11.5 It is therefore considered that the proposal would comply with Core Policy 7 of the Core Strategy and policies T2 and T8 of the Adopted Local Plan for Slough.

12.0 **Heads of Terms**

- 12.1 The applicant will need to enter into a Section 106 Agreement with Slough Borough Council; this will obligate the developer to enter into a Section 278 Agreement under the Highways Act 1980 for the satisfactory implementation of the works identified in the highways schedule.

The highways schedule includes:

- Installation of crossover / junction
- Reconstruct the footway fronting the application site
- Reinstatement of redundant access points to standard to footway construction
- Installation of street lighting modifications (as necessary)
- Drainage connections (as necessary)
- Dedication as highway maintainable at the public expense, free of charge, land within the widening line
- Lining and signing of accesses with western access as 'In' only and eastern access as 'Out' only.

12.2 A financial contribution will also be sought towards the implementation of a loading ban on the A4.

12.3 The development will also be required to achieve a BREEAM rating of 'Very Good'.

12.4 These obligations are considered to comply with Regulation 122 of the Community Infrastructure Levy Regulations 2010 in that the matters which will be covered are:

- (a) Necessary to make the development acceptable in planning terms;
- (b) Directly related to the development; and
- (c) Fairly and reasonably related in scale and kind to the development.

13.0 **Process**

13.1 In dealing with the application, the Local Planning Authority has worked with the applicant in a positive and proactive manner. The development is considered to be sustainable and in accordance with the requirements of the National Planning Policy Framework.

14.0 **Summary**

14.1 The proposal has been considered against relevant development plan policies, and regard has been had to the comments and representations received, and all other relevant material considerations.

14.2 It is recommended to delegate a decision to the Planning Manager to agree any minor amendments to the planning application, draft conditions and Section 106 planning obligation matters.

PART C: RECOMMENDATION

15.0 **Recommendation**

15.1 Delegate a decision to the Planning Manager to agree any minor amendments to the planning application, draft conditions and Section 106 planning obligation matters.

PART D: LIST OF CONDITIONS

1. The development hereby permitted shall be commenced within three years from the date of this permission.

REASON To prevent the accumulation of planning permissions, and to enable the Council to review the suitability of the development in the light of altered circumstances and to comply with the provisions of Section 91 of the Town and Country Planning Act 1990.

The development hereby approved shall be implemented only in accordance with the following plans and drawings hereby approved by the Local Planning Authority:

- (a) Drawing No. P-0001, Dated 14/DEC/14, Recd On 22/01/2015
- (b) Drawing No. P-0002, Dated 14/NOV/14, Recd On 22/01/2015
- (c) Drawing No. P-1010_PH1, Dated 14/NOV/14, Recd On 22/01/2015
- (d) Drawing No. P-1010_PH2, Dated 14/NOV/14, Recd On 22/01/2015
- (e) Drawing No. P-1015, Dated 14/NOV/14, Recd On 22/01/2015
- (f) Drawing No. P-1020, Dated 14/NOV/14, Recd On 22/01/2015
- (g) Drawing No. P-1100, Dated 14/NOV/14, Recd On 22/01/2015
- (h) Drawing No. P-1110, Dated 14/NOV/14, Recd On 22/01/2015
- (i) Drawing No. P-1120, Dated 14/NOV/14, Recd On 22/01/2015
- (j) Drawing No. P-1170, Dated 14/NOV/14, Recd On 22/01/2015
- (k) Drawing No. P-1190, Dated 14/NOV/14, Recd On 22/01/2015

REASON To ensure that the site is developed in accordance with the submitted application and to ensure that the proposed development does not prejudice the amenity of the area and to comply with the Policies in the Development Plan.

2. Samples of external materials to be used on the development hereby approved shall be submitted to and approved in writing by the Local Planning Authority before the scheme is commenced on site and the development shall be carried out in accordance with the details approved.

REASON To ensure a satisfactory appearance of the development so as not to prejudice the visual amenity of the locality in accordance with Policy EN1 of The Adopted Local Plan for Slough 2004.

3. Samples of external materials to be used in the construction of the access road, pathways and communal areas within the development hereby approved shall be submitted to and approved in writing by the Local Planning Authority before the scheme is commenced on site and the development shall be carried out in accordance with the details approved.

REASON To ensure a satisfactory appearance of the development so as not to prejudice the visual amenity of the locality in accordance with Policy EN1 of The

Local Adopted Plan for Slough 2004.

4. No development shall commence on site until a detailed landscaping and tree planting scheme has been submitted to and approved in writing by the Local Planning Authority. This scheme should include the trees and shrubs to be retained and/or removed and the type, density, position and planting heights of new trees and shrubs.

The approved scheme shall be carried out no later than the first planting season following completion of the development. Within a five year period following the implementation of the scheme, if any of the new or retained trees or shrubs should die, are removed or become seriously damaged or diseased, then they shall be replaced in the next planting season with another of the same species and size as agreed in the landscaping tree planting scheme by the Local Planning Authority.

REASON In the interests of the visual amenity of the area and accordance with Policy EN3 of The Adopted Local Plan for Slough 2004.

5. No development shall commence on site until details of the proposed boundary treatment including position, external appearance, height and materials have been submitted to and approved by the Local Planning Authority. Before the development hereby permitted is occupied, a suitable means of his boundary treatment shall be implemented on site prior to the first occupation of the development and retained at all time on the future.

REASON In the interests of the visual amenity of the area and accordance with Policy EN3 of The Adopted Local Plan for Slough 2004.

6. Prior to the first occupation of the development hereby permitted, a scheme shall be submitted to and approved in writing by the Local Planning Authority for external site lighting including details of the lighting units, levels of illumination and hours of use. No lighting shall be provided at the site other than in accordance with the approved scheme.

REASON In the interests of the amenities of the area and to comply with Core Policy 8 of The Slough Local Development Framework, Core Strategy 2006 - 2026, Development Plan Document, December 2008.

7. No development shall be begun until details of the cycle parking provision (including location, housing and cycle stand details) have been submitted to and approved in writing by the Local Planning Authority. The cycle parking shall be provided in accordance with these details prior to the occupation of the development and shall be retained at all times in the future for this purpose.

REASON To ensure that there is adequate cycle parking available at the site in accordance with Policy T8 of The Adopted Local Plan for Slough 2004, and to meet the objectives of the Slough Integrated Transport Strategy.

8. At least 10% of the energy supply of the development shall be secured from renewable and low carbon energy (as described in the glossary of the National

Planning Policy Framework). Details and a timetable of how this is to be achieved, including details of physical works on site, shall be submitted to and approved in writing to the Local Planning Authority prior to commencement of the development hereby permitted. The approved details shall be implemented in accordance with the approved timetable and retained as operational thereafter, unless otherwise agreed in writing by the Local Planning Authority.

REASON In order to comply with the requirements of Core Policy 8 of the Slough Local Development Framework, Core Strategy 2006-2026, Development Plan Document.

9. A Service Management Plan shall be submitted to and approved in writing prior to the occupation of the development hereby permitted. The Service Management Plan shall be carried out in accordance with the approved details.

REASON To ensure that adequate onsite servicing can take place and in accordance with Core Policy 8 of The Slough Local Development Framework, Core Strategy 2006-2026, Development Plan Document, December 2008.

10. The showroom and associated office use hereby permitted shall not be open to members of the public / customers outside the hours of 08:00 hours to 18:00 hours on Mondays-Thursdays, 08:00 hours to 18:00 on Fridays and Saturdays, and 10:00 hours to 16:00 hours on Sundays and Bank/Public Holidays.

REASON To protect the amenity of residents within the vicinity of the site from noise and disturbance in accordance with Core Policy 8 of The Slough Local Development Framework, Core Strategy 2006 – 2026, Development Plan Document, December 2008.

11. The workshop and MOT and valeting use hereby permitted shall not be open to members of the public / customers outside the hours of 08:00 hours to 18:00 hours on Mondays-Thursdays, 08:00 hours to 18:00 on Fridays and Saturdays, and at no times on Sundays and Bank/Public Holidays.

REASON To protect the amenity of residents within the vicinity of the site from noise and disturbance in accordance with Core Policy 8 of The Slough Local Development Framework, Core Strategy 2006 – 2026, Development Plan Document, December 2008.

12. There shall be no commercial deliveries visiting the site outside the hours of 0800 - 1800 Monday to Friday 08:00 - 13:00 on Saturdays and at no times on Sundays or Bank/Public Holidays.

REASON To protect the amenity of residents within the vicinity of the site from noise and disturbance in accordance with Core Policy 8 of The Slough Local Development Framework, Core Strategy 2006 – 2026, Development Plan Document, December 2008.

13. All deliveries and servicing to be undertaken from within the site; no servicing shall be undertaken from the public highway.

REASON To minimise danger, obstruction and inconvenience to users of the adjoining highway in accordance with Core Policy 7 of The Slough Local Development Framework, Core Strategy 2006 – 2026, Development Plan Document, December 2008.

14. No development shall commence until a noise management plan has been submitted to and approved in writing by the Local Planning Authority. This plan shall be implemented on site prior to the first occupation of the development and retained at all times in the future to mitigate noise to the levels agreed in the approved scheme.

REASON To protect the amenity of nearby occupiers in accordance with Core Policy 8 of The Slough Local Development Framework, Core Strategy 2006 – 2026, Development Plan Document, December 2008.

15. Unless otherwise agreed in writing, the western access shall only be utilised as an entrance to the site from Bath Road, and the eastern access shall only be utilised as an exit from the site onto Bath Road. The gate to the western access shall remain open at all times whilst the use is in operation and shall be kept free from obstruction.

REASON In the interests of the free flow of traffic and to prevent instances in queuing on Bath Road in accordance with Core Policy 7 of The Slough Local Development Framework, Core Strategy 2006 – 2026, Development Plan Document, December 2008.

16. No part of the development shall be occupied until the new means of access has been sited and laid out in accordance with the approval plans and constructed in accordance with Slough Borough Council's Design Guide.

REASON In order to minimise danger, obstruction and inconvenience to users of the highway and of the development in accordance with Core Policy 7 of the The Slough Local Development Framework, Core Strategy 2006 – 2026, Development Plan Document, December 2008.

17. No part of the development shall be occupied until the redundant access has been removed and the footway reinstated and constructed in accordance with Slough Borough Council's Design Guide.

REASON In order to minimise danger, obstruction and inconvenience to users of the highway and of the development in accordance with Core Policy 7 of the The Slough Local Development Framework, Core Strategy 2006 – 2026, Development Plan Document, December 2008.

18. No vehicle access gates, roller shutters doors or other vehicle entry barriers or control systems shall be installed without first obtaining permission in writing from the Local Planning Authority

REASON In order to minimise danger, obstruction and inconvenience to users of the

highway and of the development in accordance with Core Policy 7 of the The Slough Local Development Framework, Core Strategy 2006 – 2026, Development Plan Document, December 2008.

19. The scheme for parking, manoeuvring and the loading and unloading of vehicles shown on the submitted plans shall be laid out prior to the initial occupation of the development hereby permitted and that area shall not thereafter be used for any other purpose.

REASON To enable vehicles to draw off, park, load/unload and turn clear of the highway to minimise danger, obstruction and inconvenience to users of the adjoining highway in accordance with Core Policy 7 of the The Slough Local Development Framework, Core Strategy 2006 – 2026, Development Plan Document, December 2008.

20. No part of the development shall begin until visibility splays have been provided on both sides of the eastern access between a point 2.4 metres along the centre line of the access measured from the edge of the carriageway and a point 90 metres along the edge of the carriageway measured from the intersection of the centre line of the access. The area contained within the splays shall be kept free of any obstruction exceeding 0.6 metres in height above the nearside channel level of the carriageway.

REASON To provide adequate intervisibility between the access and the existing public highway for the safety and convenience of users of the highway and of the access in accordance with Core Policy 7 of the The Slough Local Development Framework, Core Strategy 2006 – 2026, Development Plan Document, December 2008.

21. No windows other than those hereby approved shall be formed in the rear or flank wall elevations of the development without the prior written approval of the Local Planning Authority.

REASON To minimise any loss of privacy to occupiers of adjoining residential properties in accordance with Policy H15 of The Adopted Local Plan for Slough 2004.

Appendix B

P/00176/033 - 392 Bath Road

Consultation

Comments have been received from Environmental Quality.

It has been recommended that a noise management plan is submitted. Condition 14 would require the submission of a noise management plan and this has therefore been addressed.

With regard to plant, it is recommended that this is conditioned to require details to be submitted to the Local Planning Authority prior to the commencement of the development. A further condition is therefore recommended as follows to address this:

Prior to the commencement of the development hereby approved, details of all plant and machinery to include times of operation shall be submitted to the Local Planning Authority for approval in writing. The details shall include an assessment of the plant and machinery undertaken in compliance with BS4142:2014. Once approved, the approved details shall be implemented on site and not subsequently altered without the prior permission of the Local Planning Authority.

REASON To protect local residents from nuisance caused by excessive noise in accordance with Core Policies 6 and 8 of The Slough Local Development Framework, Core Strategy 2006-2026, Development Plan Document, December 2008.

Further Information

The applicant has submitted a further statement to support the application:

“In response to the objections raised by the resident, we wish to state that potential impact on neighbours has been diligently addressed by engaging with Slough Borough Council’s Planning Department at pre-application stage and by procuring specialist advice and reports that have been submitted in support of the application. These have been duly considered, and as highlighted in the officer’s report, properly address the concerns raised in the objection. We would ensure all the recommendations and conditions for the development will be implemented and discharged in accordance with the Council’s Approval process.”

NO CHANGE TO RECOMMENDATION



Squire Furneaux

Authorised Volvo Cars
Dealer & Service Centre

Mr Jonathan Dymond
Area Team Leader
Planning & Building Service
Slough Borough Council
St Martins Place
51 Bath Road
Slough
SL1 3UF

20th April, 2015

Dear Mr Dymond

Re: Planning Ref P/00176/033

We are writing in respect of the above application following the decision of the Planning Committee to delegate the approval of this application to the Planning Manager. We can confirm our willingness to accommodate several relatively straightforward requests in connection with this other than the desire to reposition the workshop door to one of either of the side elevations.

We have explored with Meda (our architects) the possibility of acceding to this request, whilst at the same time meeting our operational needs, and also those of the manufacturer (Volvo) that we represent.

Unfortunately the best solution that would achieve the desired change would result in several issues that would be detrimental to the operation of the site and also to the viability of the scheme. These are:-

- 1 - The loss of a productive bay within the building.
- 2 - A change to the configuration to the MOT bay to an in-line arrangement which due to accessing ramp and brake tester is a far less efficient layout resulting in less potential throughput.
- 3 - The change in access to the workshop and manoeuvring required both externally to gain entry and internally to easily access the bays to load vehicles would be far more awkward and as a consequence less safe. Access from the rear in the centre as originally proposed would allow straightforward and easy access to all bays.

Cont.

4 - The franchise we represent has a vision for the future which involves a change to our previous workshop practices with a concept designed to take advantage of "leaner" more efficient operations intended to provide higher levels of personal service to our customers with less time and visits taken by them. It relies upon well planned layouts to customer reception areas, parts access and workshop layout in terms of both flow of vehicles in and out and also easy access to tools and equipment. The changes necessary to the layout could bring the suitability of this into doubt.


As a background to this application we sought an early meeting with the Planning Department in order to hopefully achieve the best acceptable outcome for all parties. Changes were incorporated into the plan to provide an attractive scheme that took into account the location in terms of both nearby businesses and residents, to allow easier vehicle access, the accommodation of potential future A4 road-widening schemes, vehicle parking etc. Significantly also and largely as a result of the meeting an early decision was taken not to even attempt to rehouse the current Accident Repair Workshop that we operate in our current premises in this proposal. This was due to a number of factors such as parking and storage requirements but also noise levels.

We consider the application we submitted took into account and addressed all the points discussed and feel that the detrimental impact on the scheme that the relocation of the rear door would create for the questionable improvement in terms of noise are very difficult for us to accept. We are happy to enter into a firm commitment to implement any noise management plan that is agreed.

We have represented Volvo at our current site in Petersfield Avenue since 1979 and been a privately owned local business and employer since well before that time. Much has changed in Slough over that period and the locations of businesses in the town. To remain competitive for the future both Volvo and ourselves have agreed that our business should be relocated closer to where most other car dealers and businesses can be found and nearer to or on a main road, If we are unable to produce a suitable, viable scheme the potential future for the business in town may be brought into question. The approval of this scheme would enable us to move forward with confidence for the future.

We would be pleased to attend the meeting on Thursday 23rd April to answer any questions that the Committee may have.

Yours sincerely 


Charles Slaughter,
Director.

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|--------------------|--|---------------|----------------------------------|
| Registration Date: | 27-Mar-2015 | Applic. No: | P/16111/000 |
| Officer: | Mr. J. Dymond | Ward: | Farnham |
| | | Applic type: | Major |
| | | 13 week date: | 26th June 2015 |
| Applicant: | Mr. Taj Sangha, Mackenzie Homes (Farnham Road) Ltd | | |
| Agent: | Mr. Phil Joyce, AKA Planning Riverbridge House, Guildford Road, Leatherhead, Surrey, KT22 9AD | | |
| Location: | Land rear of, 102-104, Farnham Road, Slough, Berkshire, SL1 4YS | | |
| Proposal: | Erection of a four storey building with under-croft comprising 14 residential flats together with associated car parking, refuse and cycle stores. | | |

Recommendation: Delegate to Planning Manager



1.0 **SUMMARY OF RECOMMENDATION**

1.1 This application has been referred to the Planning Committee for consideration as the application is for a major development.

1.2 Having considered the relevant policies set out below, the representations received from consultees and all other relevant material considerations, it is recommended to:

Delegate a decision to the Planning Manager for the signing of a satisfactory Section 106 Agreement (or unilateral planning obligation) as necessary; to agree any revised drawings; to consider any further observations from neighbours / consultees; to agree the outstanding matters referred to in the report; and to agree any minor amendments to the planning application, draft conditions and Section 106 planning obligation matters.

PART A: BACKGROUND

2.0 **Proposal**

2.1 This is a full planning application for the erection of a four storey building with under-croft comprising 14 no. residential flats together with associated car parking, refuse and cycle stores. Pre-application advice has been sought.

2.2 The building would accommodate 14 no. flats comprising 6 no. one bedroom and 8 no. two bedroom units at first, second and third floor levels.

3.0 **Application Site**

3.1 The site is located close to the junction with Farnham Road and Whitby Road.

3.2 The site of the proposed development was formerly used as a car park in connection with the office building now being converted to flats under permitted development. There would be 19 no. one bedroom and 20 no. two bedroom units within this converted building. This building comprises a three storey pitched roof L-shaped building. The car park is accessed via the shared access road off Whitby Road known as Melbourne Avenue.

3.3 There are a mixture of uses surrounding the site. The Centre, Slough Centre Nursery School and Northampton Place are located to the north of the site. To the east is Rotunda Club. To the south and west there are commercial units.

3.4 The site is located outside of Slough Town Centre and outside of Farnham Road District Centre.

3.5 The site is located within flood zone 1 and as such there would be a low probability of flood risk. The site is located outside of a Conservation Area. There are no listed buildings or protected trees in the immediate vicinity of the site.

4.0 **Site History**

4.1 Previous applications considered to be of relevance relating to former office building are as follows:

P/02627/006 RENEWAL OF EXISTING CLADDING AND FENESTRATION COMPRISING REPLACEMENT OF CURTAIN WALLING WITH 'ROCK PANEL' CLADDING SYSTEM AND OPERABLE WINDOWS.

F/02627/005 PRIOR APPROVAL FOR CONVERSION OF 1ST, 2ND AND 3RD FLOORS OF EXISTING BUILDING FROM OFFICES (CLASS B1(A)) TO RESIDENTIAL ACCOMMODATION (CLASS C3) INCLUDING 39 APARTMENTS

Prior Approval Not Required/Informatives 02-Jun-2014

5.0 **Neighbour Notification**

5.1 The Centre, Farnham Road, Slough, SL1 4UT, Slough Centre Nursery School, Buckingham Avenue East, Slough, SL1 3EA, Bankers Insurance Services, 117-119, Whitby Road, Slough, SL1 3DR, Burnham Garage Ltd, 87-89, Farnham Road, Slough, SL1 4UN, 65a, Whitby Road, Slough, SL1 3DP, Inspector Of Taxes District 1, Inland Revenue, 102-104, Farnham Road, Slough, SL1 4YS, Inspector Of Taxes District 2, Inland Revenue, 102-104, Farnham Road, Slough, SL1 4YT, Collector Of Taxes, Inland Revenue, 102-104, Farnham Road, Slough, SL1 4YR, 113, Whitby Road, Slough, SL1 3DR, Rotunda Youth & Community Centre, Northampton Avenue, Slough, SL1 3BP, Flat 1 - 57, Northampton Place, 82, Northampton Avenue, Slough, SL1 3FT, 63, Whitby Road, Slough, SL1 3DP, Hayward Services Ltd, 115, Whitby Road, Slough, SL1 3DR, Graham Builders, Unit 2, 91-97, Farnham Road, Slough, SL1 4UN, Norland Managed Services, 115, Whitby Road, Slough, SL1 3DR, Yoda Investments Ltd, 115, Whitby Road, Slough, SL1 3DR, Topps Tiles, Unit 1, 91-97, Farnham Road, Slough, SL1 4UN, Kwik-fit, Unit 4, 91-97, Farnham Road, Slough, SL1 4UN, Floors To Go, Unit 3, 91-97, Farnham Road, Slough, SL1 4UN, Managers Office, Northampton Place, 82, Northampton Avenue, Slough, SL1 3FT

In accordance with Article 13 of The Town and Country Planning (Development Management Procedure) (England) Order 2010, a site notice was displayed at the site and the application has been advertised in The Slough Express.

5.2 No representations received.

6.0 **Consultation**

6.1 Environmental Protection

No comments received. Any comments received will be reported on the Committee Amendments Sheet.

6.2 Thames Water

No comments received. Any comments received will be reported on the Committee Amendments Sheet.

6.3 Crime Prevention Design Advisor

No comments received. Any comments received will be reported on the Committee Amendments Sheet.

6.4 Contaminated Land

No comments received. Any comments received will be reported on the Committee Amendments Sheet.

6.5 Transport and Highways

No comments received. Comments will be reported on the Committee Amendments Sheet.

6.6 Tree Officer

No comments received. Any comments received will be reported on the Committee Amendments Sheet.

PART B: PLANNING APPRAISAL

7.0 **Policy Background**

7.1 The following policies are considered most relevant to the assessment of this application:

National Planning Policy Framework and the Planning Practice Guidance

The Slough Local Development Framework, Core Strategy 2006 – 2026, Development Plan Document

Core Policy 1 – Spatial Strategy
Core Policy 3 – Housing Distribution
Core Policy 4 – Housing
Core Policy 7 – Transport
Core Policy 8 – Sustainability and the Environment
Core Policy 9 – Natural and Built Environment
Core Policy 10 – Infrastructure
Core Policy 11 – Social Cohesiveness
Core Policy 12 – Community Safety

The Local Plan for Slough, Adopted March 2004

Policy EN1 – Standard of Design
Policy EN3 – Landscaping Requirements
Policy EN5 – Design and Crime Prevention
Policy H9 – Comprehensive Planning
Policy H14 – Amenity Space
Policy OSC15 – Provision of Facilities in new Residential Developments
Policy T2 – Parking Restraint
Policy T8 – Cycling Network and Facilities

Composite Local Plan – Slough Local Development Plan and the NPPF - PAS Self Assessment Checklist

Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires that applications for planning permission are determined in accordance with the development plan unless material considerations indicate otherwise. Annex 1 to the National Planning Policy Framework advises that due weight should be given to relevant policies in existing plans according to their degree of consistency with the Framework (the closer the policies in the plan to the policies in the Framework, the greater the weight that may be given).

The Local Planning Authority has published a self assessment of the Consistency of the Slough Local Development Plan with the National Planning Policy Framework using the PAS NPPF Checklist.

The detailed Self Assessment undertaken identifies that the above policies are generally in conformity with the National Planning Policy Framework. The policies that form the Slough Local Development Plan are to be applied in conjunction with a statement of intent with regard to the presumption in favour of sustainable development.

It was agreed at Planning Committee in October 2012 that it was not necessary to carry out a full scale review of Slough's Development Plan at present, and that instead the parts of the current adopted Development Plan or Slough should all be republished in a single 'Composite Development Plan' for Slough. The Planning Committee endorsed the use of this Composite Local Plan for Slough in July 2013.

Other relevant documents

Slough Local Development Framework, Site Allocations, Development Plan Document (adopted November 2010)

Slough Local Development Framework Proposals Map

Slough Borough Council Developer's Guide Parts 1-4

Guidelines for the Provision of Amenity Space Around Residential Properties (January 1990)

Guidelines for Flat Conversions (April 1992)

7.2 The main planning issues relevant to the assessment of this application are considered to be as follows:

- 1) Principle of development;
- 2) Design and Impact on the street scene;
- 3) Relationship with and potential impact on neighbouring properties;
- 4) Amenity space for residents;
- 5) Parking and highway safety.

8.0 **Principle of Development**

8.1 Core Policy 1 of the Core Strategy sets out the overarching spatial strategy for development within the Borough. This policy requires that the scale and density of development will be related to the site's current or proposed accessibility, character and surroundings.

8.2 Core Policy 4 of the Core Strategy sets out the Council's approach to the consideration of proposed housing development within the Borough. This states that in the urban areas outside the town centre, new residential development will predominantly consist of family housing and be at a density related to the character of the surrounding area, the accessibility of the location, and the availability of existing and proposed local services, facilities and infrastructure.

8.3 Given the location of the site, outside of Slough Town Centre, Core Policy 4 of the Core Strategy would therefore normally require the provision of family housing.

8.4 It is however acknowledged that the suitability of the site for the provision of family housing may be somewhat limited, given the relationship of the site to the Centre and the general character of the area.

8.5 The proposal is for the construction of 14 no. flats immediately next to a building currently being converted under permitted development. The number of flats proposed under the permitted development conversion is 39 no.

8.6 The proposed building is lower than the existing building and is of a smaller footprint. In this context, it is considered that the scale and density of the proposed development is related to the site's current or proposed accessibility, character and surroundings.

8.5 The total number of flats proposed, the mixture and size of units is considered to be acceptable in this location. The principle of the proposal is considered to be acceptable.

9.0 **Design and Impact on the Street Scene**

- 9.1 Policy EN1 of the Adopted Local Plan for Slough and Core Policy 8 of the Core Strategy requires that the design of proposed residential development should be of a high standard of design and reflect the character and appearance of the surrounding area.
- 9.2 The proposed building would be located to the north eastern corner of the site. The ridge height of the proposed building would be lower than that of the existing building. It is considered that the scale, massing and appearance would be acceptable and inkeeping with the character and appearance of the existing building and surrounding area.
- 9.3 With regard to proposed materials, these would be similar in appearance to the existing building and would comprise red facing brickwork together with white and grey cladding. These are considered to be acceptable.
- 9.4 The size, layout and stacking of the proposed flats is considered to be acceptable. It is considered that habitable rooms would have good outlook and aspect. A daylight and sunlight assessment has been undertaken. The results of the analysis show that future occupiers will benefit from good levels of lighting.
- 9.5 A landscaping strategy has been proposed which includes the retention of, and provision of new tree planting to the frontages to Melbourne Avenue. Additional planting is also proposed along the Farnham Road frontage. The proposal also incorporates security measures. This is considered to be acceptable.
- 9.6 The proposal is considered to be acceptable in design and street scene terms.
- 10.0 **Relationship with and Potential Impact on Neighbouring Properties**
- 10.1 It is considered that the main area for consideration in relation to the potential impact on neighbouring occupiers would be with respect to the potential impact of the proposed development on future occupiers of the flats within the building currently being converted under permitted development; and the potential impact on future development on adjacent sites.
- 10.2 Core Policy 8 of The Core Strategy states that all development will be of a high quality and respect its location and surroundings.
- 10.3 **Impact on Future Occupiers of Converted Building**
- 10.4 The existing former officer building is currently being converted to flats under permitted development. Windows serving habitable rooms would face the proposed building.
- 10.5 The separation distance between the opposing elevations of the existing and proposed building would be 22 metres. A 21 metre separation distance would normally be sought in such situations. It is considered that the proposal would not likely have the potential to have an unacceptable adverse impact on future occupiers of the flats within the building currently being converted under permitted development in terms of loss of light, overshadowing or overlooking.
- 10.6 **Impact On Future Development On Adjacent Sites**
- 10.7 Policy H9 of the Adopted Local Plan for Slough states that a comprehensive approach should be taken in any residential development scheme to ensure that adjoining land which is capable of development is not sterilised.
- 10.8 The Council's Asset Management section have confirmed that Slough Borough Council as

neighbouring land owner of the Rotunda Club and The Centre have redevelopment proposals for land to the north and the east of the application site. They have objected to the proposal on the grounds of prejudicing efficient development on adjoining land to the east.

- 10.9 The applicant has sought to ensure that separation distances to the north and east have been maintained, as follows:
- The separation distance between the proposed development and the northern boundary would be 12.5 metres;
 - The separation distance between the proposed development and the centre line of Melbourne Avenue would be 10 metres.
- 10.10 It is understood that The Centre site is to be redeveloped to provide leisure uses. The car park to the east of the site, on the opposite side of Melbourne Avenue could potentially provide housing.
- 10.11 There are no windows proposed on the northern elevation of the proposed building and it is not considered that the proposal will have the potential to impact on the redevelopment of The Centre.
- 10.12 Turning to the relationship with the car park to the east, the applicant has maintained a separation distance of 10 metres to the centre line of Melbourne Avenue, on the understanding that any development on the adjacent car park would similarly have to maintain a reasonable separation distance in the interests of ensuring that their site is not adversely impacted and a comprehensive approach is taken.
- 10.13 As noted above, normally, a 21 metre minimum separation distance between primary elevations is required and it is considered that there is scope for this to be achieved and that adjoining land which is capable of being development would not be sterilised.
- 10.14 On this basis, it is considered that the proposed development would be acceptable and would comply with Core Policy 8 of the Core Strategy and the National Planning Policy Framework.
- 11.0 **Amenity Space for Residents**
- 11.1 Whilst the proposal does not include private amenity space, the applicant has indicated that they are agreeable to mitigation in the form of a contribution towards the enhancement of existing nearby public open space. Slough Borough Council Developer's Guide Part 2 sets out that a financial contribution in lieu of private amenity space can be sought in the case of high density residential schemes that have inadequate private amenity space.
- 12.0 **Parking and Highway Safety**
- 12.1 Core Policy 7 of the Core Strategy sets out the Planning Authority's approach to the consideration of transport matters. The thrust of this policy is to ensure that new development is sustainable and is located in the most accessible locations, thereby reducing the need to travel.
- 12.2 Policy T2 of The Adopted Local Plan for Slough 2004 seeks to restrain levels of parking in order to reduce the reliance on the private car through the imposition of parking standards.
- 12.3 The application is supported by a Transport statement. The Council's Highway and Transport consultant has been consulted and comments will be provided on the Committee Amendments Sheet.

- 12.4 The proposed site access formed with Melbourne Avenue would be shared by both buildings as would the car parking area. The site would be provided with a total of 74 no. car parking spaces for the use of the proposed and converted blocks. 16 no. car parking spaces would be located within the proposed under-croft, the remaining spaces would be around the buildings.
- 12.5 A total of 74 no. car parking spaces are proposed to serve both the 39 no. units within the converted building forming part of the prior approval and the 14 no. units proposed. The applicant submits that this level of parking provision complies with the Council's parking standards. The table below has been provided to show the parking demand across the site:

| Flat Size | No. of Units | Parking Standard | Parking Required | Parking Proposed |
|------------------------|--------------|------------------|------------------|------------------|
| Proposed Scheme | | | | |
| 1 bed | 6 | 1 | 6 | 6 |
| 2 bed | 8 | 1.75 | 14 | 14 |
| | | Sub Total | 20 | 20 |
| Converted Block | | | | |
| 1 bed | 19 | 1 | 19 | 19 |
| 2 bed | 20 | 1.75 | 35 | 35 |
| | | Sub Total | 54 | 54 |
| | | TOTAL | 74 | 74 |

13.0 **Planning Obligations**

- 13.1 Core Policy 10 of the Core Strategy states that development will only be allowed where there is sufficient existing, planned or committed infrastructure. All new infrastructure must be sustainable. Where existing infrastructure is insufficient to serve the needs of new development, the developer will be required to supply all reasonable and necessary on-site and off-site infrastructure improvements.

13.2 **Affordable Housing and Education**

- 13.3 On sites of 1 hectare or 25 dwellings or more, 30% of dwellings in a development shall normally be social housing to meet those in most need as defined by the Council. The Developer's Guide sets out that in the case of developments comprising 15 or more dwellings, a financial contribution for education would be sought for each dwelling.
- 13.4 The proposal is for the construction of 14 no. flats. This would fall below the normal 15 no. unit threshold where contributions for education and affordable housing would normally be sought. No affordable housing or education contributions are therefore required to mitigate the proposed development.

13.5 **Amenity Space**

- 13.6 A contribution per flat will be sought towards the improvement of public open space nearby the site.

13.7 **Highways and Transport**

- 13.8 The need for highways and transport contributions is under consideration and any comments regarding these matters will be reported on the Committee Amendments Sheet.

14.0 **Process**

- 14.1 In dealing with the application, the Local Planning Authority has worked with the applicant in a positive and proactive manner. Pre-application advice has been provided and amendments

have been undertaken to the proposed development. The development is considered to be sustainable and in accordance with the requirements of the National Planning Policy Framework.

15.0 **Summary**

- 15.1 The proposal has been considered against relevant development plan policies, and regard has been had to the comments received, and all other relevant material considerations.

PART C: RECOMMENDATION

16.0 **Recommendation**

- 16.1 Delegate a decision to the Planning Manager for the signing of a satisfactory Section 106 Agreement (or unilateral planning obligation) as necessary; to agree any revised drawings; to consider any further observations from neighbours / consultees; to agree the outstanding matters referred to in the report; and to agree any minor amendments to the planning application, draft conditions and Section 106 planning obligation matters.

PART D: CONDITIONS

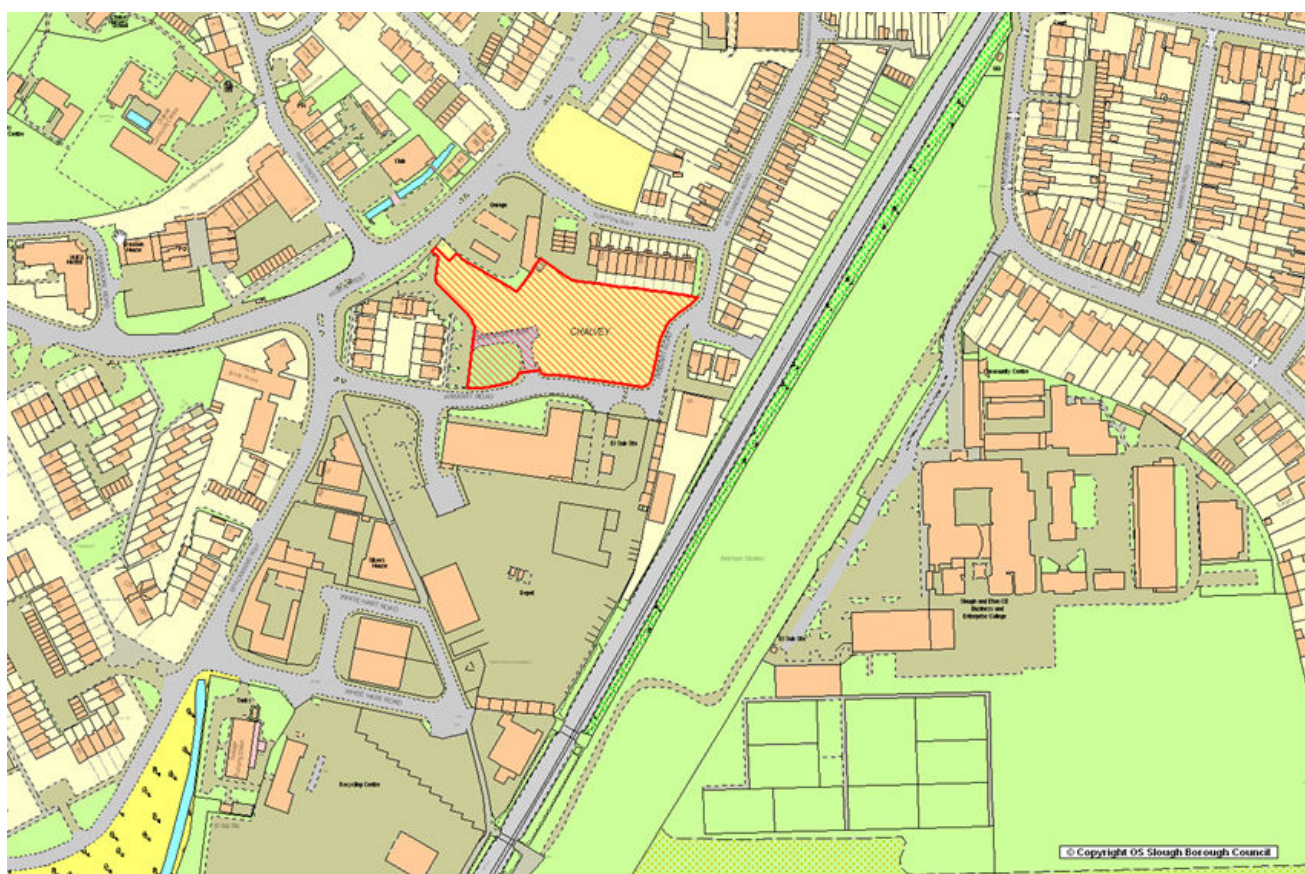
The following summary of conditions is proposed:

1. Commence within three years from the date of this permission
2. Approved plans and drawings
3. Samples of external materials
4. Landscape & boundary treatment
5. Secured by Design
6. Details of wall and floor sound insulation to be submitted
7. Details of the cycle parking provision to be submitted
8. Details of the proposed bin stores to be submitted
9. No future occupier of the flats hereby approved shall be entitled to a car parking permit
10. Details of electric vehicle charging points
11. Number of car parking spaces to be provided and made available for use
12. Car parking spaces shall only be used by residential occupiers and for no separate business or commercial use
13. Pedestrian visibility
14. Waste Minimisation Plan
15. Construction Management Plan
16. Drainage details
17. Land contamination
18. No impact piling shall take place until a piling method statement has been submitted
19. Hours of construction
20. Hours of deliveries
21. Minimum distance of gates from highway
22. Reinstate redundant crossovers
23. Details of access

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| | | | |
|--------------------|--|---------------|---------------------------------|
| Registration Date: | 09-Mar-2015 | Applic. No: | P/00322/019 |
| Officer: | Mr. Albertini | Ward: | Chalvey |
| | | Applic type: | Major |
| | | 13 week date: | 8th June 2015 |
| Applicant: | Dr. Hemantha Kumar, The Bharani Medical Centre | | |
| Location: | Greenwatt Way, Slough, Berkshire, SL1 3SJ | | |
| Proposal: | Outline application for 60 extra care flats in a 4 storey building & full planning permission for a medical hub in a 3 storey building | | |

Recommendation: Delegate to Planning Manager



1.0 **SUMMARY OF RECOMMENDATION**

Delegate to Planning Manager for completion Section 106 planning obligation.

PART A: BACKGROUND

2.0 **Proposal**

- 2.1 This is an outline planning application for a 60 extra care assisted living flats in a 4 storey building and a full planning application for a medical hub in a 3 storey building. The hub will be at the west, narrow, end of the site and the extra care flats at the east end next to Primary Road. Both will be accessed off the existing Greenwatt Way which in turn leads off Primary Road (which is off Spackmans Way).
- 2.2 The medical hub will be a doctors surgery but have capacity and flexibility to provide additional clinical services, minor operations and social services provision devolved from mainstream providers. It will have 8 consulting rooms plus 2 for health care assistants and 2 treatment rooms for minor operations. Total floorspace of the medical hub will be 1,260 sqm.
- 2.3 The entrance and reception area, which extends out at ground floor level, from the bulk of the building, is on the east end of the building. The rest of the ground floor is undercroft car parking for 17 staff plus cycle storage. 16 customer parking spaces are proposed mostly in a parking area between Greenwatt Way and Primary Road but in front of the hub building. The building is close to the adjacent petrol station boundary but turns its back on this site.
- 2.4 The flat roofed building will be finished with blue-grey brick at ground floor level with striped panels above. The main elevations will have a definite horizontal emphasis with the end elevations a vertical emphasis. The end elevations will feature a recess but will also project beyond the ground floor. Panel colour details have not been provided at this stage. The main south elevation will have a projecting bay at first floor level. At ground floor level a curved wall will encompass a projecting bay and the entrance area with the upper floors projecting out beyond it. Window patterns follow the vertical or horizontal emphasis and at lower level increase in size around the entrance area.
- 2.5 The extra care development will include day care and support services. It is designed to give the elderly and infirm an ability to extend their independent lifestyle whilst feeling reassured that care is immediately on hand. The building will include a restaurant, activity and social amenity spaces. 52 one bedroom and 8 two bedroom flats are proposed.
- 2.6 For this outline application the applicant wishes details of access, layout and scale (mass of the building) to be considered at this stage. Appearance and landscaping are to be Reserved Matters.
- 2.7 The U shaped building partly encloses a garden area on the south side over which some flats have an outlook. Communal and service spaces on the ground floor are mainly on the north side of the building with an entrance on the west side. Landscape areas are shown to the east and alongside part of the west wing.
- 2.8 The 4 storey building steps back above the ground floor on the elevation facing existing maisonettes to the north. Indicative information has been submitted to show how 60 units can be accommodated in the mass of the building and how windows relate to nearby homes in terms of overlooking. On the third floor, the building opens up for a potential roof garden with a conservatory corridor link. The building has a shallow pitched roof.

- 2.9 12 car parking spaces are provided off a service road at the west end of the building and linking to the enclosed service yard north of the building.
- 2.10 To support the application the applicant has submitted a design and access statement, transport assessment, flood risk assessment and ground investigation report. Regarding surface water the applicant has clarified that drainage will be by soakaway or similar on site involving clearance of contaminated soil.
- 2.11 The transport assessment concludes that the development will generate more peak hour trips but at such a low level there would be no detrimental impact on the adjacent road network. It also points out the sustainable location of the site being accessible by non-car modes of travel.

3.0 **Application Site**

- 3.1 This 0.52 hectare site has been vacant since demolition of the old Southern Electric office and depot a few years ago. To the west are 10 two storey homes built as zero carbon homes by SSE. Some of these homes face the site. Beyond, on the other side of Chlavey High Street, is the Working Mens Club. To the north of the proposed hub is a petrol station. To the north of the extra care flats are a three storey block of maisonettes and small garage court on Turton Way. The maisonettes have 6 metre gardens and first first floor balconies. To the east, beyond Primary Road, are 2 storey homes and an industrial unit of Alexandra Road to the south east. To the South is SSEs new office and depot. The site incorporates the grassed area at the entrance to Greenwatt Way.
- 3.2 The site is about a mile from the town centre and close to Chalvey shops and local facilities. There are some shrubs and small trees on the north boundary.
- 3.3 The west boundary of the site abuts Environment Agency flood zone 3 which is an area liable to flood. The ground investigation reports points out there is some contamination of the site but remediation is practical.

4.0 **Site History**

- 4.1 10 homes (and Greenwatt Way) on adjacent site approved Dec 2009 (P/00322/017).

Outline permission for 48 homes Approved Sep 2009 (P/322/12) (this included the above 10 homes site)

5.0 Neighbour Notification

- 5.1 Turton Way 10 – 48 ev
Alexandra Road 49 – 65 odd
Greenwatt Way 1 – 10 inclusive
High Street (Chalvey) Petrol Stn; 48, 50,52
Primary Road; SSE
- 5.2 No observations received.

6.0 **Consultation**

- 6.1 Traffic /Highways

Full comments to be on amendment sheet. Travel Plan needed to encourage non car modes of travel.

6.2 Environmental Protection

Comments to be on amendment sheet. Standard conditions expected.

6.3 Drainage

Highlights that connection to main sewer not practical but drainage on site is dependent upon soil being sufficiently decontaminated.

PART B: PLANNING APPRAISAL

7.0 **Policy Background**

- 7.1 The Local Plan identifies this site as an existing business area. An exception to the policy of retaining business use has already been accepted by way of the previous outline planning permission for residential development and relocation of SSEs old office from the site. Use of part of the site for the medical hub is supported as this will provide an important local community facility. Development of the site is also supported to assist regeneration and improve the appearance of the area. This is in line with the Core Policy strategic objectives regarding regeneration and enhancing local community benefits.

8.0 **Transport and Access**

- 8.1 Greenwatt Way was designed to allow further development to take place so the access is acceptable. Pedestrians can access the site direct from High St. Chalvey via the end of Greenwatt Way just beyond the petrol station. The service road for the extra care development could potentially access a small Council owned site to the rear if that is incorporated into the application site in the future and not redeveloped independently off Turton Way.
- 8.2 A Travel Plan and associated financial contribution for monitoring will be needed to encourage non car modes of travel. This is intended to help mitigate the extra traffic generated compared to previously proposed residential scheme for the site. Any further issues or need for off site works or contributions etc. will be reported on the meeting amendment sheet.
- 8.3 The level of car parking proposed for the medical hub is acceptable 17 spaces for staff and 16 for patients. The location of the site is reasonably suitable for non car modes of travel. For the extra care flats it is assumed no residents would have a car. The 12 spaces provided are 3 spaces below the Council's published standards. Additional spaces have been requested. Adjacent to the site Primary Road, which is a cul de sac, might get used as for overflow kerb side parking space.
- 8.4 Cycle parking is in the secure undercroft parking area for the hub plus racks for patients near the entrance door. By condition cycle parking for the flats development will be required. Overall the proposal complies with Core Policy 7 transport.

9.0 **Design and Layout Matters**

- 9.1 The medical hub is 10 and 16 metres from the nearest Greenwatt Way homes but the orientation of the latter is such that there will be no significant overlooking into windows.
- 9.2 The extra care flats building will be relatively close to Turton Way flats for the size of building. The bulk of the 4 storey building will be 20.1 metres away from windows in the ground floor of Turton Way flats and 22.75 metres from upstairs windows (these are set

back). 2 stair wells will protrude out from part of the elevation creating a 16.6 metre distance to ground floor windows. At ground floor level the kitchen and service area will extend out to within 3 metres of the site boundary and 9.4 metres of the ground floor elevation of existing homes.

- 9.3 The separation distances above are reasonable in terms of levels of light to existing homes although, compared to now, those homes will notice a change and residents might feel as if the new building is a bit overbearing. The flats at the far east end of Turton Way will be less affected as the proposed building does not line up exactly with the row of flats.
- 9.4 As this is an outline application window detail is not put forward for approval at this stage. However if the size and location of the building are being decided at this stage it is important to know that an acceptable window arrangement is possible without causing unacceptable overlooking problems for adjacent homes. Indicative floor plans and north elevation windows have been submitted to show that the proposed size and location are reasonably acceptable for an urban location.
- 9.5 A 20 metre separation distance between habitable rooms is achievable. 21 metres would normally be requested as a minimum. Bearing in mind the short gardens of the adjoining homes and the size of the building, as described above, it is appropriate to minimise any further affect on living conditions. The indicative information about the arrangement of rooms in the extra care scheme submitted shows that it is possible to have no habitable rooms in the elevation that is next to the Turton Way flats. Corridor or bathroom/kitchen windows can be on the elevation. It will be important that, at the next Reserved Matters application stage, the indicative scheme is adhered to in terms of minimising overlooking and not being taller or closer to the flats.
- 9.6 Separation distances to homes in Alexandra Road are acceptable. Flats at the front will be 19 and 20 metres from the SSE office building opposite.
- 9.7 When going down Chalvey High Street the medical hub building will be visible behind the petrol station canopy but will be set back several metres from the road compared to the Greenwatt Way homes beyond. As it is a community facility it is appropriate for it to be obvious in the streetscene and the set back ensures it will not obscure the distinctive white Greenwatt way homes. The extra care building is well away from Chalvey High Street but because of its height it will be visible from that road behind the petrol station.
- 9.8 The details of the appearance of the extra care building are not part of this application. The submitted details for the medical hub are acceptable. The horizontal emphasis of the detailing at upper level on the long building and use of cladding means it will be quite a striking building that will contrast with the white rendered homes of Greenwatt Way and typical domestic architecture of Chalvey homes in the vicinity. However the nearby commercial buildings provide a context that is varied in terms building size and appearance such that the building can be considered acceptable.
- 9.9 The shrubs and small trees on the north boundary of the vacant site next to the petrol station are likely to conflict with the construction of the development but their loss is not significant. Trees on the boundary of the Turton Way flats can hopefully be retained subject to a check on their health. Retention of existing or new small trees at this location will be important to help soften the appearance of the new building for existing residents.
- 9.10 The proposal is in accordance with Local Plan design and landscaping policy EN1 and EN2 and Core Strategy policy 8 and 9 re sustainability, environment and the built environment.

10 **Section 106 Matters**

- 10.1 For the development to be acceptable a Section 106 Planning Obligation is needed for the approval of a Travel Plan and payment of an associated monitoring fee.

PART C: RECOMMENDATION

11 **Recommendation**

- 11.1 Delegate a decision to the Planning Manager

- for the signing of a satisfactory Section 106 Agreement
- to agree revised drawings requested
- to agree any minor amendments to the planning application, draft conditions and Section 106 planning obligation matters.

12 **PART D: LIST OF CONDITIONS.**

1. Time Limit Extra Care Apartments (Outline)

Application for approval of all reserved matters referred to in condition 3 below shall be submitted in writing to the Local Planning Authority no later than the expiration of 30 months from the date of this permission. The outline permission covers the land identified on drawing 48 14 P 11 by a dotted redline.

The development hereby permitted must be begun not later than whichever is the later of the following dates and must be carried out in accordance with the reserved matters approved:

- i) the expiration of 3 years from the date of this permission: or
- ii) the expiration of two years from the final approval of the reserved matters referred to in condition 3 below, or in the case of approval of different dates, the final approval of the last such matter to be approved.

REASON To prevent the accumulation of planning permissions, and to enable the Council to review the suitability of the development in the light of altered circumstances and to comply with the provisions of Section 92 (2) of the Town and Country Planning Act 1990.

2. Time Limit full planning permission medical hub

The development hereby permitted (medical hub as identified on drawing number 48 14 P 11 by a solid red line) shall be commenced within three years from the date of this permission.

REASON To prevent the accumulation of planning permissions, and to enable the Council to review the suitability of the development in the light of altered circumstances and to comply with the provisions of Section 91 of the Town and Country Planning Act 1990.

3. Outline applications - Reserved Matters.

Details of the external appearance and landscaping of the site (hereinafter collectively referred to as 'the reserved matters') shall be approved in writing by the Local Planning Authority prior to the commencement of the development.

REASON To ensure that the proposed development is satisfactory and to comply with the provisions of Article 6 of The Town and Country Planning (Development Management Procedure) (England) Order 2015.

4. Approved plans

The development hereby approved shall be implemented only in accordance with the following plans and drawings hereby approved by the Local Planning Authority:

48_14_P_01 Location Plan
48_14_P_10 Site Layout
48_14_P_11 Site Layout Application Type Boundaries Recvd April 2015
48_14_P_20 Medical Hub Ground Floor Plan
48_14_P_21 Medical Hub First Floor Plan
48_14_P_22 Medical Hub Second Floor
48_14_P_23 Medical Hub Roof Plan
48_14_P_24 Medical Hub elevations
48_14_P_25 Medical Hub elevations
48_14_P_26 Medical Hub sections
48_14_P_27 Medical Hub Elevations context

48_14_P_30 Extra Care Apts Ground Floor Plan
48_14_P_31 Extra Care Apts First Floor
48_14_P_32 Extra Care Apts Second Floor
48_14_P_33 Extra Care Apts Third Floor
48_14_P_34 Extra Care Apts Roof Plan
48_14_P_35 Extra Care Apts Massing Elevations
48_14_P_36 Extra Care Apts Massing Elevations
48_14_P_37 Extra Care Apts Massing Elevations Extended
48_14_P_38 Extra Care Apts Section

REASON To ensure that the site is developed in accordance with the submitted application and to ensure that the proposed development does not prejudice the amenity of the area and to comply with the Policies in the Development Plan.

5. Details of materials medical hub

Details of external materials and samples of bricks and cladding to be used on the development (medical hub as defined on drawing 48 14 P 11) hereby approved shall be submitted to and approved in writing by the Local Planning Authority before the scheme is commenced on site and the development shall be carried out in accordance with the details approved.

REASON To ensure a satisfactory appearance of the development so as not to prejudice the visual amenity of the locality in accordance with Policy EN1 of The Adopted Local Plan for Slough 2004.

6. Details of materials extra care apartments

Details of external materials and samples of bricks and cladding to be used on the development (extra care apartments as defined on drawing 48 14 P 11) hereby approved shall be submitted to and approved in writing by the Local Planning Authority before the scheme is commenced on site and the development shall be carried out in

accordance with the details approved.

REASON To ensure a satisfactory appearance of the development so as not to prejudice the visual amenity of the locality in accordance with Policy EN1 of The Adopted Local Plan for Slough 2004.

7. Landscaping Scheme medical hub

No development (medical hub development as identified on drawing 48 14 P 11) shall commence on site until a detailed landscaping and tree planting scheme has been submitted to and approved in writing by the Local Planning Authority. This scheme should include the trees and shrubs to be retained and/or removed and the type, density, position and planting heights of new trees and shrubs.

The approved scheme shall be carried out no later than the first planting season following completion of the development. Within a five year period following the implementation of the scheme, if any of the new or retained trees or shrubs should die, are removed or become seriously damaged or diseased, then they shall be replaced in the next planting season with another of the same species and size as agreed in the landscaping tree planting scheme by the Local Planning Authority.

REASON In the interests of the visual amenity of the area and accordance with Policy EN3 of The Adopted Local Plan for Slough 2004.

8. Landscaping Scheme extra care apartments

No development (extra care apartments development as identified on drawing 48 14 P 11) shall commence on site until a detailed landscaping and tree planting scheme has been submitted to and approved in writing by the Local Planning Authority. This scheme should include the trees and shrubs to be retained and/or removed and the type, density, position and planting heights of new trees and shrubs.

The approved scheme shall be carried out no later than the first planting season following completion of the development. Within a five year period following the implementation of the scheme, if any of the new or retained trees or shrubs should die, are removed or become seriously damaged or diseased, then they shall be replaced in the next planting season with another of the same species and size as agreed in the landscaping tree planting scheme by the Local Planning Authority.

REASON In the interests of the visual amenity of the area and accordance with Policy EN3 of The Adopted Local Plan for Slough 2004.

9. Bin and cycle stores for the medical hub

The bin stores shown on the approved plans shall be available for use prior to the occupation of the medical hub building. The cycle stores shown on the approved plans shall be available for use prior to the occupation of the medical hub building and shall include cycle stands details of which shall have first been submitted to and been approved in writing by the local planning authority.

REASON In the interest of public health, visual amenity and encouraging non car modes of travel.

10. Bin and cycle Stores Extra Care Apartments

No construction work on the extra care apartments development (as identified on drawing 48 14 P 11) shall commence until the location and details of bin stores and cycle stores and cycle stands have first been submitted to and been approved in writing by the local planning authority. The cycle stores and bin stores shall be available for use as approved prior to the occupation of the extra care apartments building.

REASON In the interest of public health, visual amenity and encouraging non car modes of travel.

11. Internal access roads and parking

Prior to first occupation of the development, the internal access roads footpath and turning provision shall be provided in accordance with approved plans. Prior to first occupation of each building of the development the car parking associated with respective buildings shall be provided in accordance with approved plans.

REASON To ensure that the proposed development does not prejudice the free flow of traffic or conditions of general safety on the local highway network in accordance with Policy T3 of The Adopted Local Plan for Slough 2004.

12. New access

No development shall commence until details of the new means of access are submitted to and approved in writing by the Local Planning Authority and the access shall be formed, laid out and constructed in accordance with the details approved prior to occupation of the development.

REASON To ensure that the proposed development does not prejudice the free flow of traffic or conditions prejudicial of general safety along the neighbouring highway in accordance with Policy T3 of The Adopted Local Plan for Slough 2004.

13. Surface Water Drainage

No development shall commence until a surface water drainage strategy has been submitted and approved in writing by the local planning authority.

REASON In the interest of flood prevention and coordination of drainage.

14. Surface water drainage medical hub

No development (of the medical hub) shall commence until details of surface water drainage have been submitted to and been approved in writing by the local planning authority. Drainage shall be implemented in accordance with the approved details before first occupation of the medical hub building.

REASON In the interest of flood prevention.

15. Surface water drainage extra care apartments

No development (of the extra care apartments) shall commence until details of surface water drainage have been submitted to and been approved in writing by the local planning authority. Drainage shall be implemented in accordance with the approved details before first occupation of the extra care apartments.

REASON In the interest of flood prevention.

16. Soil Contamination - TO BE DRAFTED

17. Sustainable Development - TO BE DRAFTED

18. Hours of construction

During the demolition / construction phase of the development hereby permitted, no work shall be carried out on the site outside the hours of 08.00 hours to 18.00 hours Mondays - Fridays, 08.00 hours - 13.00 hours on Saturdays and at no time on Sundays and Bank/Public Holidays.

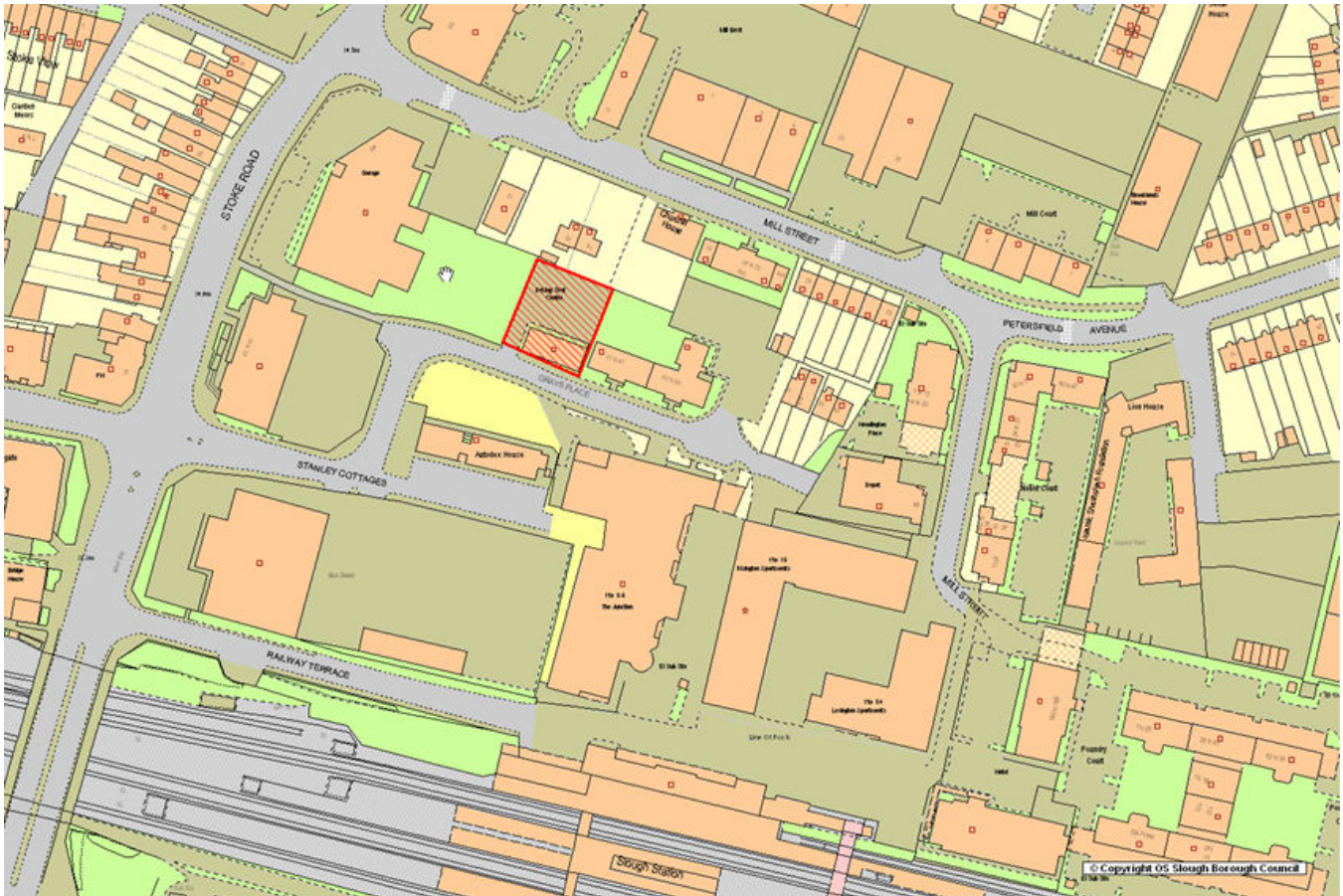
REASON To protect the amenity of residents within the vicinity of the site in accordance with Core Strategy policy 8 the objectives of Policy EN26 of The Adopted Local Plan for Slough 2004.

INFORMATIVE(S):

1. The applicant is reminded that an Agreement under Section 106 of the Town and Country Planning Act 1990 has been entered into with regards to the application hereby approved.
2. HIGHWAY MATTERS - TO BE DRAFTED
3. In dealing with this application, the Local Planning Authority has worked with the applicant in a positive and proactive manner through pre-application discussions. It is the view of the Local Planning Authority that the proposed development does improve the economic, social and environmental conditions of the area for the reasons given in this notice and it is in accordance with the National Planning Policy Framework.

| | | | |
|--------------------|---|---------------|----------------------------------|
| Registration Date: | 30-Mar-2015 | Applic. No: | P/16122/000 |
| Officer: | Mr Smyth | Ward: | Central |
| | | Applic type: | Major |
| | | 13 week date: | 29th June 2015 |
| Applicant: | Ms. Shazia Shah, Adil Property Investments Ltd | | |
| Agent: | Mr. Eshan UL-Haq, ArchiGrace 50, Two Mile Drive, Slough, SL1 5UH | | |
| Location: | Driving Standards Agency, Driving Test Centre, Grays Place, Slough, SL2 5AF | | |
| Proposal: | Construction of 3.5 storey high building to provide 14no. flats (including accommodation in the roofspace) plus landlords office and basement to provide storage and facilities for residents, on site parking for 10no cars and 14 bicycles plus refuse store. | | |

Recommendation: Delegated to Planning Manager



1.0 **SUMMARY OF RECOMMENDATION**

- 1.1 Delegate the planning application to the Planning Manager for the consideration of outstanding consultations, any minor design changes, finalising conditions and final determination.
- 1.2 Having considered the relevant Policies below, the development is considered not to have an adverse affect on the sustainability and the environment for the reasons set out.

PART A: BACKGROUND

2.0 **Proposal**

- 2.1 The application proposes construction of 3.5 storey high building to provide 14no. flats (including accommodation in the roofspace) plus landlords office and basement to provide storage and facilities for residents, on site parking for 10 no cars and 14 bicycles plus refuse store.
- 2.2 The building measures 8.2m to eaves (12m to ridge, as amended) X 16m deep narrowing to 12.25m at the eastern end X 22m wide narrowing to 19m at the northern end.
- 2.3 At basement level, there are additional residents ancillary facilities together with storage lockers for each of the proposed flats and separate plant area. The basement has both stairs and lift access.
- 2.4 Ground floor comprises 2 no. offices, one measuring 49.06 sq m the other measures 65.58 sq m. The offices sit either side of the gated entrance, providing both vehicular and pedestrian access to the rear parking court and main entrance door, which located on the west side of the building. There is a separate pedestrian footpath to the side of the building providing direct access to the main entrance.
- 2.5 The first and second floors each have 5 no flats comprising 3 no. X 2 bedroom flats and 2 no. X 1 bedroom flats. The third floor within the roof space contains 4 no. flats comprising 1 no X 2 bedroom flat and 3 X 1 bedroom flats. The stairs to the roof space accommodation are provided via a dormer projection within the north west elevation.
- 2.6 The gated vehicle access provides vehicular access undercroft of the building to a rear parking court containing 10 no. car parking spaces. The rear main wall of the building is sited 9 metres from the boundary with the existing houses at 16 and 18 Mill Street to the north. An illustrative plan has been submitted to show that the site comprising 16 & 18 Mill Street is capable of being developed whilst maintaining a similar distance from its boundary to the south with an overall separation distance between the buildings of 18 metres.

3.0 **Application Site**

- 3.1 This site is situated to the north of Grays Place and is currently occupied by a single storey portakabin building with associated car park and is being used as a driving test centre. The site is currently leased and it is understood that the current lease will fall due in August 2015.
- 3.2 The site is within an area of mixed uses comprising commercial and residential properties. Adjoining the site to the west is a disused commercial car sales site, to the south is a 4 storied scheme, to the north are 2 no. dilapidated residential properties and to the east is a

block of three storey flats. The site lies within an area which is experiencing gradual change and regeneration. The immediate area has heights of 2 or 3 stories although the buildings are taller around the railway station which is further to the south, but step down to the above mentioned heights in this location.

- 3.3 The site is located within the Slough Town Centre Area but is not located within the Commercial Core

4.0 **Site History**

- 4.1 There is no relevant planning history for this site. However, pre application planning advice was sought and the guidance given has for the most part been taken on board as part of the planning submission, however, further changes have been negotiated during the assessment of this application.

5.0 **Neighbour Notification**

- 5.1 Neighbours Consulted: The Occupier, 31 – 59 (odds), Grays Place, Slough
- Cullen Burns Associates Ltd, Automotive House
Grays Place, Slough
- Roman House, Grays Place, Slough
- The Occupier, Flats 1 - 133, The Junction, Grays Place, Slough
- The Occupier, 20-28 Mill Street, Slough
SL2 5DH
- The Occupier, 18, Mill Street, Slough, SL2 5DH
- The Occupier, 16, Mill Street, Slough, SL2 5DH
- The Occupier, Thames Pathology Services, 12, Mill Street, Slough, SL2 5DH
- Press Notice Placed in Local Press and On Site
- NO COMMENTS RECEIVED

6.0 **Consultation**

- 6.1 Transport & Highways

Any comments received will be reported on the Amendment Sheet

- 6.2 Land Contamination

Any comments received will be reported on the Amendment Sheet

- 6.3 Drainage

Given the proposals for a fully excavated basement, the development would require attenuation in the form of above ground storage, to ensure an acceptable soakage. Details of surface water drainage can be covered by planning condition.

PART B: PLANNING APPRAISAL

7.0 Policy Background

7.1 This application is assessed against the following national and local planning policies:

- National Planning Policy Framework & Planning Practice Guidance
- Core Policies, 1, 4, 7 8 and 12 of the Slough Local Development Framework Core Strategy (2006 – 2026) Development Plan Document December 2012
- Policies H14, EN1, EN3, EN5, and T2 of the Adopted local Plan for Slough

7.2 The application is assessed in accordance with the following:

- Principle of Development
- Design and Street Impact
- Impact on Neighbouring Occupiers/Uses
- Land and Groundwater Contamination
- Transport, Access, Servicing and Parking
- Drainage and Flood Risk
- Quality of Housing
- Landscaping & Amenity Space

8.0 Principle of Development

8.1 At the heart of the NPPF is a presumption in favour of sustainable development which should be seen as a “golden thread running through both plan making and decision taking”. In respect of decision taking this means inter alia approving development proposals that accord with the development plan without delay.

Twelve core planning principles are identified which both should underpin plan making and decision taking. A number of these core principles are relevant to the current proposals being:-

- Always seek to secure a quality design and a good standard of amenity for all existing and future occupants of land and buildings
- Support the transition to a low carbon future in a changing climate, taking full account of flood risk, the reuse of existing resources and the encouragement for using renewable resources
- Encourage the effective use of land by reusing land that has previously been developed, provided that it is not of high environmental value
- Actively manage patterns of growth to make the fullest possible use of Public Transport, walking and cycling, and focus significant development to locations which are or can be made sustainable.

At paragraph 49 in respect of delivering a wide choice of high quality homes it states that housing applications should be considered in the context of the presumption in favour of sustainable development.

8.2 Core Policy 1 sets out the overall spatial strategy for Slough requiring all developments to take place within the built up area, predominately on previously developed land. The policy seeks to ensure high density housing is located in the appropriate parts of Slough Town Centre with the scale and density of development elsewhere being related to the sites current or proposed accessibility, character and surroundings.

Core Policy 4 again emphasises that high density housing should be located in the Town

Centre area and that outside the Town Centre the development will be predominately family housing at a density related to the character of the area. In particular, in suburban residential areas, there will only be limited infilling consisting of family houses which are designed to enhance the distinctive suburban character and identity of the area. The site is not identified as a development site within the Slough Local Development Framework Site Allocation Document DPD.

- 8.3 Being located within the Town Centre Area, the provision of flats complies with Core Policies 1 and 4 of the LDF Core Strategy.
- 9.0 Design and Street Scene Impact
- 9.1 The National Planning Policy Guidance, in its overarching Core Planning principles state that planning should: *Proactively drive and support sustainable economic development to deliver the homes, business and industrial units infrastructure and thriving local places that the country needs.....always seek to ensure high quality design and a good standard of amenity for all existing and future occupants of land and buildingshousing applications should be considered in the context of the presumption in favour of sustainable development.....good design is a key aspect of sustainable development, is indivisible from good planning and should contribute positively to making places better for people.*
- 9.2 Core Policy 8 states that all development in the borough shall be sustainable, of a high quality design, improve the quality of the environment and address the impact of climate change. With respect to achieving high quality design all development will be:
- a) be of a high quality design that is practical, attractive, safe, accessible and adaptable
 - b) respect its location and surroundings
 - c) be in accordance with the Spatial Strategy in terms of its height, scale, massing and architectural style
- 9.3 Policy EN1 of the adopted Local Plan requires that development proposals reflect a high standard of design and must be compatible with and/or improve their surroundings in terms of: scale, height, massing, bulk, layout, siting, building form and design, architectural style, materials, access points and servicing, visual impact, relationship to nearby properties, relationship to mature trees, and relationship to water courses.
- 9.4 The application site is located within an area which is undergoing change and regeneration, as evidenced by other flatted developments which have taken place or are taking place on sites north of Slough Railway Station. The site cries out for comprehensive redevelopment as part of a wider area which includes properties adjoining the site to the south and fronting onto Mill Street. However, given the complexities of ownership in the area, it is unlikely that development agreements could be secured in the immediate future.
- 9.5 The site has a frontage to Grays Place which is a short cul de sac, comprising a mix of residential and commercial development. On the opposite side of Grays Place there is a 3 storey commercial building. To the east of that building is a four storey flatted development with ground floor car parking undercroft of the building. This scales up to an adjacent 6/7 storey flatted scheme, to its west. Adjoining the site to the east there is a three storey flatted scheme. Beyond that are a number of two storey houses. To the west of the site is a vacant building which formerly operated as a car show room.

9.6 Whilst the proposed development is marginally higher than the existing three storey flats to the east, it is considered that the development fits in well with other recent developments in Grays Place which have been built to heights of between 5 – 7 floors equivalent.

9.7 The building is designed to be traditional in its appearance using a mix of stone dressing and brick. Proposals for vehicle access under the building has been used a means of accessing a rear/undercroft car park replicating residential developments opposite. The building has a hipped and pitched roof reflecting the flats adjacent to the east.

9.8 No objections are raised on grounds of design or street scene impact.

10.0 **Impact on Neighbouring Occupiers/Uses**

10.1 As stated above, in the National Planning Policy Statement Twelve core planning principles are identified which both should underpin plan making and decision taking. A number of these core principles are relevant to the current proposals and include that planning should:

- *Always seek to secure a quality design and a good standard of amenity for all existing and future occupants of land and buildings.*

Core Policy 8 of the Slough Local Development Framework Core Strategy states that all development in the borough shall be sustainable, of a high quality design, improve the quality of the environment and address the impact of climate change. With respect to achieving high quality design all development will:

a) respect its location and surroundings.

Policy EN1 requires that development proposals reflect a high standard of design and must be compatible with and/or improve their surroundings in terms of.....*relationship to nearby properties*.....

10.2 The development has been designed with a side /rear stagger to avoid breaching any line of sight from the rear of neighbouring flats at 31 – 41 Grays Place. The development will result in some loss of sunlight for the neighbouring flats during the late afternoon and evening periods, although this would not be so significant as to warrant a refusal of planning permission. There will some impact when viewed from the communal rear gardens belonging to those flats, although being communal rather than private amenity space, this is not considered to be a significant issue.

10.3 At ground floor level flank wall windows are proposed for the offices, although these are secondary windows provided for light and can be conditioned to be obscurely glazed and fixed shut or high level opening only. At first and second floor levels, in the eastern elevation there are windows serving a kitchen and in the western elevation there are flank wall windows serving a kitchen and stairwell. Conditions will be imposed requiring these windows to be obscurely glazed and either fixed shut or high level opening only. At third floor level within the roofspace there are no windows proposed within either side roof plane.

10.4 To the rear of the site are a pair of semi detached houses (nos 16 & 18 Mill Street), both of which are in a poor state of repair. No 18 Mill Street is being used for a commercial car wash. No 16 is still understood to be occupied. Both sites are ripe for redevelopment. So as not to prejudice redevelopment of the neighbouring site, through sterilisation by direct overlooking, the applicants have provided illustrative plans showing how the site could be redeveloped following redevelopment of the application site. A notional window to window distance of 18 metres can be achieved between the development currently under

consideration and a potential redevelopment on this neighbouring site. This approach assumes a minimum distance of 9 metres set off from the rear boundaries of each respective site. It further assumes redevelopment of both sites to provide flatted development which does not require the provision of private amenity space. A condition will be imposed requiring that no part of the building (above ground floor) be permitted within 9 metres when measured from the rear boundary of the site.

- 10.5 Subject to the imposition of appropriate conditions no objections are raised on grounds of impact.

11.0 **Land & Groundwater Contamination**

- 11.1 Core Policy 8 of the Core Policy 8 of the Slough Local Development Framework Core Strategy states development shall not:

b) Cause contamination or a deterioration in land soil or water quality.

- 11.2 Further guidance will be provided via the Amendment Sheet.

12.0 **Transport, Access, Servicing and Parking**

- 12.1 Core Policy 7 of the Slough Development Framework Core Strategy states that: *All new development should reinforce the principles of the transport strategy.....and should improve road safety.....*

Policy T2 of the Adopted Local Plan for Slough states that: residential development will be required to provide a level of parking appropriate to its location and which will overcome road safety problems, protect the amenities of adjoining residents and not result in an adverse visual impact upon the environment.....

- 12.2 Preliminary verbal comments have been provided by the transport and highway engineers. This will be reported more fully on the Amendment Sheet.

Car Parking

As the site is located within the town centre but outside of the commercial core there would be an expectation that some on site car parking would be provided. In general terms car parking at a ratio of 1 space per flat has been accepted on sites even outside of the town centre area.

As part of this application 10 no. car parking spaces are proposed which equates to 0.7 spaces per dwelling. This is considered acceptable given the site's location which is highly sustainable within easy walking distance of both the bus and rail stations and the town centre which its range of shops and services.

Provision is made for secure cycle parking on site sufficient to accommodate 14 no. bicycles, albeit the storage area may need to be marginally increased in size to ensure adequate access to all of the cycle stands.

Future occupiers are to be restricted from taking up residents parking permits and this will be covered by planning condition.

Refuse

Provision is made for a separate access to the refuse store from Grays Place. The drag distance is @ 12 metres which marginally exceeds the guideline of 10 metres although this would not be sufficient grounds to warrant a refusal of planning permission.

Access

Pedestrian and vehicular access is proposed from Grays Place. The vehicular access is gated. The gates are set back 4.5m from the highway which is less than the normal requirement of 6 metres. However, the highway engineers do not propose to object in this instance. The existing access will need to be reinstated to footway and this would be covered as part of a minor highway works agreement.

- 12.3 No objections are raised on grounds of access or parking subject to minor design changes to the cycle store and conditions restricting future occupiers from taking up parking permits.

13.0 **Drainage and Flood Risk**

- 13.1 In the Technical Guidance to the NPPF, the guidance sets out the following:

As set out in the National Planning Policy Framework, inappropriate development in areas at risk of flooding should be avoided by directing development away from areas at highest risk, but where development is necessary, making it safe without increasing flood risk elsewhere. For these purposes:

“areas at risk of flooding” means land within Flood Zones 2 and 3; or land within Flood Zone 1 which has critical drainage problems and which has been notified to the local planning authority by the Environment Agency;

“flood risk” means risk from all sources of flooding - including from rivers and the sea, directly from rainfall on the ground surface and rising groundwater, overwhelmed sewers and drainage systems, and from reservoirs, canals and lakes and other artificial sources

- 13.2 Core Policy 8 of the Core Policy 8 of the Slough Local Development Framework Core Strategy states:

a) Development will only be permitted where it is safe and it can be demonstrated that there is minimal risk of flooding to the property and it will not impede the flow of floodwaters, increase the risk of flooding elsewhere or reduce the capacity of a flood plain; and

b) Development must manage surface water arising from the site in a sustainable manner which will also reduce the risk of flooding and improve water quality.

- 13.3 The site is not located within either flood zones 2 or 3, nor is it located within an identified critical flood risk area within flood zone 1. In terms of managing surface water disposal, this will necessitate on site mitigation/storage to achieve an run off rate of 5 litres per second. Planning conditions will be imposed requiring details of drainage to be submitted for approval.

14.0 **Quality of Housing**

- 14.1 The National Planning Policy Framework requires that local planning authorities ensure the provision of a wide range of good quality homes.

- 14.2 As a guide to internal room sizes the Council relies on its planning guidelines for flat conversions.

| Unit | Lounge/Kitchen en 1 bed(20.43 sq m) | Bedroom Bed 1 (11.14 sq m) Bed 2 (6.5 sq m) | Complies Y | Complies N |
|------|--|--|---------------|---------------|
| | | | | |

| | | | | |
|---------------|----------------------|----------------|--------|--|
| | 2 bed(22.29 sq m) | | | |
| 1 (2 bed) | 24.7 | 11.58 7.39 | Y Y | |
| 2 (1 bed) | 23.16 | 11.46 | Y | |
| 3 (2 bed) | 26.42 | 13.94 11.27 | Y Y | |
| 4 (2 bed) | 23.12 | 11.60 6.93 | Y Y | |
| 5 (1 bed) | 29.52 | 13.01 | Y | |
| 6 (2 bed) | 24.7 | 11.58 7.39 | Y Y | |
| 7 (1 bed) | 23.16 | 11.46 | Y | |
| 8 (2 bed) | 26.42 | 13.94 11.27 | Y Y | |
| 9 (2 bed) | 23.12 | 11.60 6.93 | Y Y | |
| 10 (1 bed) | 29.52 | 13.01 | Y | |
| 11 (1 bed) | 25.09 | 13.72 | Y | |
| 12 (1 bed) | 25.27 | 12.11 | Y | |
| 13 (2 bed) | 23.12 | 11.60 6.93 | Y Y | |
| 14 (1 bed) | 31.97 | 13.01 | Y | |

It is apparent from the above table that all room sizes comply with the Council's guidance.

14.3 It is accepted that all habitable rooms have an acceptable aspect and would benefit from good daylight. The south facing flats would benefit from direct sunlight. In addition there are no room stacking issues with for the most part like rooms being above like rooms. However, any noise transmission issues can be resolved at the building regulations stage.

14.4 No objections are raised on grounds of housing quality.

15.0 **Landscaping and Amenity Space**

15.1 Policy H14 of the adopted local plan recognises that the demand for gardens within flatted schemes is less strong than for individual family houses. The ground floor is for the most part occupied by surface car parking, with little real opportunity to provide soft landscaping. Balconies are provided for all south facing ground and first floor flats. The remaining flats have Juliette balconies. Given the site's town centre location this level of provision is considered to be acceptable and as such no objections are raised.

16.0 **S106 Contributions**

16.1 As the scheme falls below the 15 no. dwelling threshold, there are no requirements for

affordable housing, education or amenity space contributions.

PART C: RECOMMENDATION

17.0 Recommendation

Delegate the planning application to the Planning Manager for the consideration of outstanding consultations, any minor design changes, finalising conditions and final determination

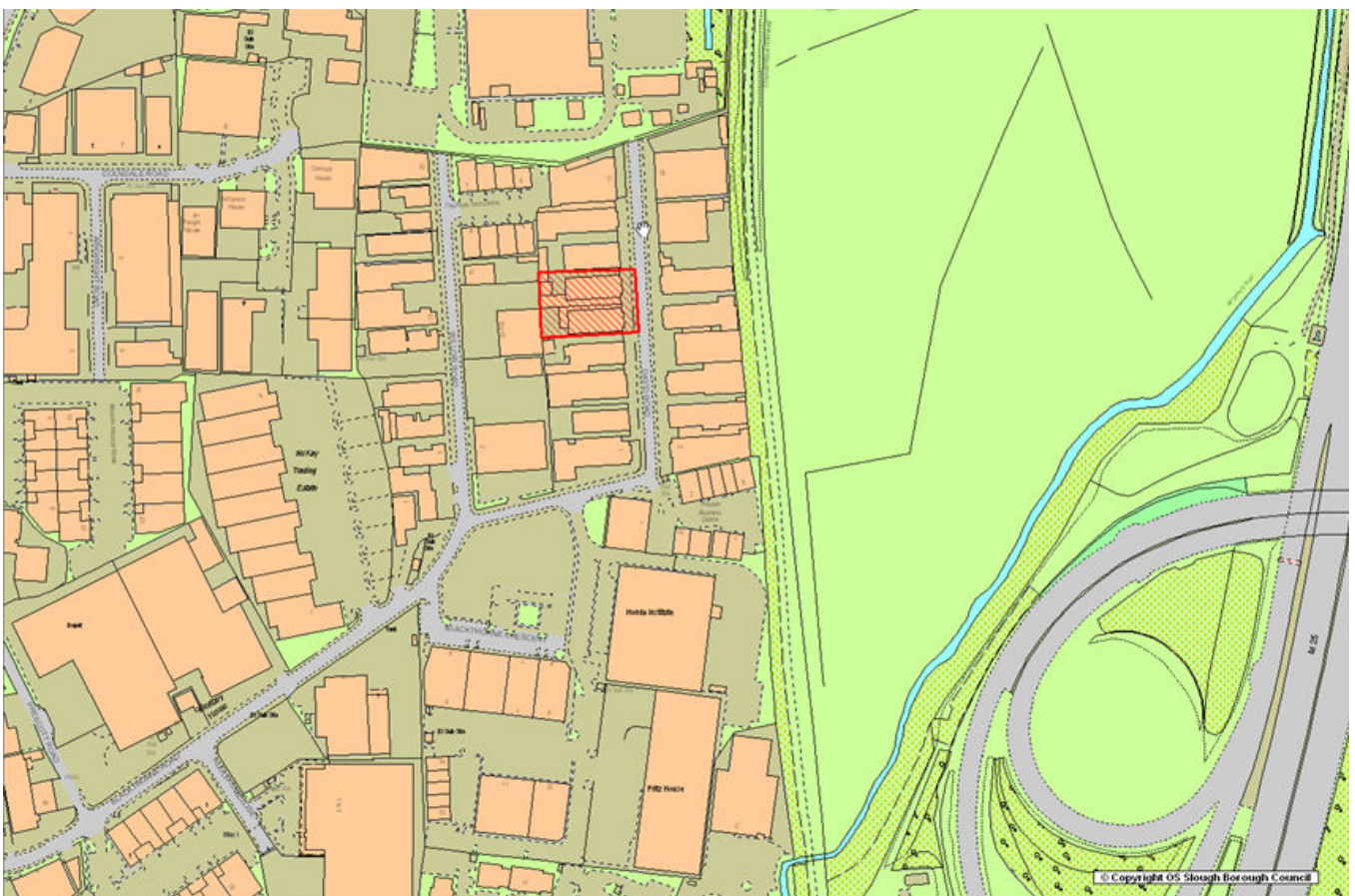
18.0 Conditions

18.1 The following summary of conditions is proposed:

- Time
- Approved drawings
- Samples of Materials
- Surface Materials
- Landscape & Boundary Treatment
- Minimum Car Parking
- Details of Bin Store & Secure Cycle Parking
- Pedestrian Visibility
- Waste Minimisation Plan
- Construction Management Plan
- Drainage Strategy
- Minimum Distance to Rear Boundary
- Obscure Glazing & High Level Opening Flank Wall Windows.
- Details of Residents Amenities to be Provided in Basement
- Land Contamination Study
- Hours of Construction
- Hours of Deliveries
- Minimum Distance of Gates from Highway
- Reinstate Redundant Crossovers.
- Details of Access
- External Lighting
- Restriction on Residents Parking Permits
- Electric Car Charging Points

| | | | |
|--------------------|--|---------------|----------------------|
| Registration Date: | 02-Apr-2015 | Applic. No: | P/16138/000 |
| Officer: | Mr Smyth | Ward: | Colnbrook with Poyle |
| | | Applic type: | Major |
| | | 13 week date: | 2nd |
| Applicant: | Miwa Ltd | | |
| Agent: | Mr. L Huntley, Woods Hardwick Ltd 15-17, Goldington Road, Bedford, MK40 3NH | | |
| Location: | 9-11, DAVID ROAD, POYLE TRADING ESTATE, COLNBROOK, SLOUGH, BERKSHIRE, SL3 0DB | | |
| Proposal: | Demolition of existing buildings and construction of a new two storey building for use as a store and preparation centre in connection with the neighbouring bakery. | | |

Recommendation: Delegated to the Planning Manager



1.0 **SUMMARY OF RECOMMENDATION**

- 1.1 Delegate the planning application to the Planning Manager for resolution of outstanding highway issues, consideration of outstanding consultations, any minor design changes, finalising conditions completion of a S106 Agreement and final determination.
- 1.2 Having considered the relevant Policies below, the development is considered not to have an adverse affect on the sustainability and the environment for the reasons set out.

PART A: BACKGROUND

2.0 **Proposal**

- 2.1 This application is for demolition of the two existing buildings on site and redevelopment to provide a two storey building for use as a store and preparation centre in association with the neighbouring bakery. The main part of the building measures 30.6m deep X 39.3m wide plus there is a forward projection measuring 11.5m X 27.5m. The building is 10 metres high stepping down to 8.25m in relation to the front projection. The building has a gross internal floorspace of 3000 sq m.
- 2.2 The proposal includes frontage car parking for 14 no. cars, including 2 no. disabled spaces via a new central vehicle crossover. Parking for HGV parking is provided on site which would be capable of accommodating 2 no. lorries, but with space on site to allow 4 no. 10 metre long rigid vehicles to be able to wait. The vehicles would turn using the adjacent highway.
- 2.3 The front of the building contains primary entrance, staff entrance offices and staff welfare facilities.
- 2.4 The Transport Technical Note advises that the maximum number of persons on site at any one time would be 30.
- 2.5 The application is accompanied by existing and proposed site plans, floor plans and elevations. In addition the following supporting statements are submitted:
- Design and Access Statement
 - Planning Statement
 - Transportation Technical Note

3.0 **Application Site**

- 3.1 The site which measures approximately 0.22 hectare is situated within the existing Business Area of Lakeside Road, Galleymead Road and Poyle Industrial estate. The site is surrounded by industrial/commercial buildings, many of which are dated in appearance. The site is currently occupied by two separate flat roof buildings which in the past have been used for light engineering and are at present being used for storage and office accommodation.
- 3.2 The northern western and southern boundaries are occupied by existing commercial buildings. The applicants own both nos 9 & 11 David Road, which comprises the application site and the neighbouring building at no. 13 David Road which is being retained.

4.0 **Site History**

- 4.1 There is no relevant planning history for the site. The site is not an allocated site with the Slough Local Development Framework Site Allocations Document. Pre application planning advice has been given for this site.

5.0 **Neighbour Notification**

- 5.1 Neighbours Consulted: N F T Logistics Ltd, David House, 1, David Road, Colnbrook,
Kangaroo International Express, David House, 1, David Road, Colnbrook,
Avenue Tools Ltd, 3, David Road, Colnbrook
A G M Group, 5, David Road, Colnbrook
Wagstaff Foundries Ltd, 7, David Road, Colnbrook
Hunt Base, 15, David Road, Colnbrook
Decor 11 Ltd, 15, David Road, Colnbrook
The Occupier, 17, David Road, Colnbrook
G D S K 40635, David Road, Colnbrook
Ascot Metal Finishers Ltd, 6, David Road, Colnbrook
Hankoe Stove Enamelling Co Ltd, 6, David Road, Colnbrook
The Occupier, 8, David Road, Colnbrook
The Occupier, 10, David Road, Colnbrook
The Occupier, 10a David Road, Colnbrook
P A E DESIGN SERVICES, 12, David Road, Colnbrook
Hawk Freight Services Ltd, 1 Poyle Technical Centre, Willow Road Colnbrook
AIR COURIER INTERNATIONAL, 2 Poyle Technical Centre Willow Road, Colnbrook
Capital Incorporation Services, 3 Poyle Technical Centre Willow Road, Colnbrook
Atlas Packing Services Ltd, 4 Poyle Technical Centre, Willow Road Colnbrook
5 Poyle Technical Centre, Willow Road, Colnbrook
G N K Freight Services Ltd, 6 Poyle Technical Centre, Willow Road Colnbrook
Star Freight Ltd, 7 Poyle Technical Centre, Willow Road Colnbrook
8 Poyle Technical Centre, Willow Road, Colnbrook
The Occupier, 14, Willow Road, Colnbrook
The Occupier, Unit 8, Willow Road, Poyle Industrial Estate Colnbrook
The Occupier, 10, Willow Road, Colnbrook
The Occupier, 14-16, David Road, Colnbrook
The Occupier, 13, David Road, Colnbrook

Notice Placed on Site and in Local Press

NO OBJECTIONS RECEIVED

6.0 **Consultation**

6.1 Land Contamination Officer

I have reviewed the information submitted for the above property, as well as our records related to potential contaminative land uses at the property and within 100m of the property.

Historical mapping indicates the following potential contaminative historical land uses on-site and within 100m of the enquiry site:

On site:

- **Warehouse** – located at no. 9 David Road, the site has been identified as having a potential low ranking risk as part of the Council's prioritisation procedure, but it is

not currently considered for future site investigation.

- **Engineering Works** – located at no. 11 David Road, the site has been identified as having a potential medium ranking risk as part of the Council's prioritisation procedure, but it is not currently considered for future site investigation.

Off-site:

- **Engineering Works** – located approximately 17m to the east of the enquiry site, has been classified as having a potential medium ranking risk as part of the council's prioritisation procedure, but it is not currently considered for future site investigation.
- **Joinery Works** – located approximately 17m to the east of the enquiry site, has been classified as having a potential low ranking risk as part of the council's prioritisation procedure, but it is not currently considered for future site investigation.
- **Longford (2) Poyle Landfill**– located approximately 80m further to the east of the enquiry site, has been classified as having a potential low ranking risk as part of the council's prioritisation procedure, but it is not currently under Waste Management License.
- **Engineering Works** – located approximately 25m to the north-east of the enquiry site, has been classified as having a potential medium ranking risk as part of the council's prioritisation procedure, but it is not currently considered for future site investigation.
- **Engineering Works** – located approximately 50m further to the north-east of the enquiry site, has been classified as having a potential medium ranking risk as part of the council's prioritisation procedure, but it is not currently considered for future site investigation.
- **Metal Works** – located adjacent to the north of the enquiry site, has been classified as having a potential medium ranking risk as part of the council's prioritisation procedure, but it is not currently considered for future site investigation.
- **Scaffolding Depot** – located approximately 20m further to the north of the enquiry site, has been classified as having a potential low ranking risk as part of the council's prioritisation procedure, but it is not currently considered for future site investigation.
- **Racing Car Manufacturers** – located 40m further to the north of the enquiry site, has been classified as having a potential low ranking risk as part of the council's prioritisation procedure, but it is not currently considered for future site investigation.
- **Upholstery Works** – located approximately 37m to the north-west of the enquiry site, has been classified as having a potential medium ranking risk as part of the council's prioritisation procedure, but it is not currently considered for future site investigation.
- **Engineering Works** – located adjacent to the west of the enquiry site, has been classified as having a potential medium ranking risk as part of the council's prioritisation procedure, but it is not currently considered for future site investigation.
- **Plastic Works** – located adjacent to the west of the enquiry site, has been classified as having a potential low ranking risk as part of the council's prioritisation procedure, but it is not currently considered for future site investigation.

- **Engineering Works** – located adjacent to the west of the enquiry site, has been classified as having a potential low ranking risk as part of the council's prioritisation procedure, but it is not currently considered for future site investigation.
- **Units use unmarked** – located approximately 10m to the south-west of the enquiry site, has been classified as having a potential low ranking risk as part of the council's prioritisation procedure, but it is not currently considered for future site investigation.
- **Laboratories** – located approximately 45m further to the south-west of the enquiry site, has been classified as having a potential low ranking risk as part of the council's prioritisation procedure, but it is not currently considered for future site investigation.
- **Engineering Works** – located adjacent to the south of the enquiry site, has been classified as having a potential low ranking risk as part of the council's prioritisation procedure, but it is not currently considered for future site investigation.
- **Electronics Works** – located approximately 20m further to the south of the enquiry site, has been classified as having a potential low ranking risk as part of the council's prioritisation procedure, but it is not currently considered for future site investigation.
- **Engineering Works** – located 40m further to the south of the enquiry site, has been classified as having a potential low ranking risk as part of the council's prioritisation procedure, but it is not currently considered for future site investigation.
- **Units use unmarked** – located approximately 50m further to the south of the enquiry site, has been classified as having a potential low ranking risk as part of the council's prioritisation procedure, but it is not currently considered for future site investigation.

Based on the above information the applicant is notified with regards to the off-site presence of a landfill and four historical tanks in the vicinity of the enquiry site, which may constitute a potential source of volatile contaminants. This may result in the migration of volatile contaminants into the proposed development and via inhalation pathways to the human health receptors (residents).

Therefore, following conditions shall be placed on the application:

1. Phase 1 Desk Study

Development works shall not commence until a Phase 1 Desk Study has been submitted to and approved in writing by the Local Planning Authority. The Phase 1 Desk Study shall be carried out by a competent person in accordance with Government, Environment Agency and Department for Environment, Food and Rural Affairs (DEFRA) guidance and approved Codes of practices, including but not limited to, the Environment Agency model procedure for the Management of Land Contamination CLR11 and Contaminated Land Exposure Assessment (CLEA) framework, and CIRIA Contaminated Land Risk Assessment Guide to Good Practice C552. The Phase 1 Desk Study shall incorporate a desk study (including a site walkover) to identify all potential sources of contamination at the site, potential receptors and potential pollutant linkages (PPLs) to inform the site preliminary Conceptual Site Model (CSM).

REASON: To ensure that the site is adequately risk assessed for the proposed development and in accordance with Policy 8 of the Core Strategy 2008.

2. Phase 2 Intrusive Investigation Method Statement

Should the findings of the Phase 1 Desk Study approved pursuant to the Phase 1

Desk Study condition identify the potential for contamination, development works shall not commence until an Intrusive Investigation Method Statement (IIMS) has been submitted to and approved in writing by the Local Planning Authority. The IIMS shall be prepared in accordance with current guidance, standards and approved Codes of Practice including, but not limited to, BS5930, BS10175, CIRIA 665 and BS8576. The IIMS shall include, as a minimum, a position statement on the available and previously completed site investigation information, a rationale for the further site investigation required, including details of locations of such investigations, details of the methodologies, sampling and monitoring proposed.

REASON: To ensure that the type, nature and extent of contamination present, and the risks to receptors are adequately characterised, and to inform any remediation strategy proposal and in accordance with Policy 8 of the Core Strategy 2008.

3. Phase 3 Quantitative Risk Assessment and Site Specific Remediation Strategy

Development works shall not commence until a quantitative risk assessment has been prepared for the site, based on the findings of the intrusive investigation. The risk assessment shall be prepared in accordance with the Contaminated Land report Model Procedure (CLR11) and Contaminated Land Exposure Assessment (CLEA) framework, and other relevant current guidance. This must first be submitted to and approved in writing by the Local Planning Authority and shall as a minimum, contain, but not limited to, details of any additional site investigation undertaken with a full review and update of the preliminary Conceptual Site Model (CSM) (prepared as part of the Phase 1 Desk Study), details of the assessment criteria selected for the risk assessment, their derivation and justification for use in the assessment, the findings of the assessment and recommendations for further works. Should the risk assessment identify the need for remediation, then details of the proposed remediation strategy shall be submitted in writing to and approved by the Local Planning Authority. The Site Specific Remediation Strategy (SSRS) shall include, as a minimum, but not limited to, details of the precise location of the remediation works and/or monitoring proposed, including earth movements, licensing and regulatory liaison, health, safety and environmental controls, and any validation requirements.

REASON: To ensure that potential risks from land contamination are adequately assessed and remediation works are adequately carried out, to safeguard the environment and to ensure that the development is suitable for the proposed use and in accordance with Policy 8 of the Core Strategy 2008.

4. Remediation Validation

No development within or adjacent to any area(s) subject to remediation works carried out pursuant to the Phase 3 Quantitative Risk Assessment and Site Specific Remediation Strategy condition shall be occupied until a full validation report for the purposes of human health protection has been submitted to and approved in writing by the Local Planning Authority. The report shall include details of the implementation of the remedial strategy and any contingency plan works approved pursuant to the Site Specific Remediation Strategy condition above. In the event that gas and/or vapour protection measures are specified by the remedial strategy, the report shall include written confirmation from a Building Control Regulator that all such measures have been implemented.

REASON: To ensure that remediation work is adequately validated and recorded, in the interest of safeguarding public health and in accordance with Policy 8 of the Core Strategy 2008.

6.2 Transport and Highways

No comments received to date, but comments were provided at the pre application advice

stage. Any additional comments received will be reported on the Amendment Sheet.

6.3 Colnbrook with Poyle Parish Council

No Objections Raised

6.4 Heathrow Safeguarding

We have now assessed the application against safeguarding criteria and can confirm that we have no safeguarding objections to the proposed development.

However, we would like to make the following observation:

Cranes

Given the nature of the proposed development it is possible that a crane may be required during its construction. We would, therefore, draw the applicant's attention to the requirement within the British Standard Code of Practice for the safe use of Cranes, for crane operators to consult the aerodrome before erecting a crane in close proximity to an aerodrome. This is explained further in Advice Note 4, 'Cranes and Other Construction Issues' (available at <http://www.aoa.org.uk/policysafeguarding.htm>)

PART B: PLANNING APPRAISAL

7.0 **Policy Background**

7.1

National guidance

- National Planning Policy Framework and Technical Guidance Notes.
- National Planning Practice Guidance.

The NPPF states that unless material considerations dictate otherwise development proposals that accord with the development plan should be approved without delay. That planning should not act as an impediment to sustainable growth and should avoid the long term protection of sites allocated for employment use where there is no reasonable prospect of a site being used for that purpose. It also states that high quality design should be secured and a good standard of amenity for all existing and future occupants of land and buildings. Permission should be refused for development of poor design that fails to take the opportunities available for improving the character and quality of an area and the way it functions.

Local Development Framework, Core Strategy 2006-2026, Development Plan Document

- Core Policy 1 (Spatial Vision and Strategic Objectives for Slough)
- Core Policy 5 (Employment)
- Core Policy 7 (Transport)
- Core Policy 8 (Sustainability & the Environment)
- Core Policy 10 (Infrastructure)

Adopted Local Plan for Slough

- EN1 (Standard of Design)
- EMP2 (Criteria for Business Developments)
- EMP9 (Lakeside Road Estate, Gallymead Road and the Poyle Estate)
- T2 (Parking Restraint)

7.2 The development is assessed in relation to the following:

- Principle of Development
- Design and Street Scene Impact

- Impact on Neighbouring Occupiers/Uses
- Transport, Access and Parking
- Flood Risk , Drainage and Land Contamination
- S106

8.0 **Principle of Development**

8.1 The site is located within an established Business Area and is already in commercial use. The proposal to use the building for Class B2 General Industrial development falls within the range of uses permitted within the Business Area as set out in Policy EMP9 of the adopted Local Plan for Slough.

8.2 No objections are raised as to the principle of the development in relation to the NPPF, LDF Core Strategy and adopted Local Plan, subject to satisfying all of the detailed considerations as set out below.

9.0 **Design and Street Scene Impact**

9.1 David Road is characterised by two storey brick built industrial buildings, most with flat roofs, which for the most part are dated in appearance. The current proposal will occupy 2 no. sites introducing a building of much larger scale and proportion to others in the street. Whilst the scale and height of the building, its appearance and finishing would not reflect what already exists, there is an expectation that other sites are likely to come forward in the future for redevelopment to provide units which are more likely to meet the space requirements and general expectations of modern day companies.

9.2 Whilst the proposed building would not neatly tie in with existing buildings within David Road, Poyle industrial estate has been undergoing change during recent years, particularly in the growth of wholesale warehousing and distribution, reflecting its location close to Heathrow Airport. Gradual renewal is a trend which is likely to continue here, unless the wider industrial estate is adversely affected by a future decision in relation to the siting of a third runway at Heathrow airport. It is also noted that David Road does not have a strong public realm, being located within the heart of an existing industrial estate and being a cul de sac without any obvious pedestrian permeability. As such no objections are being raised on grounds of design or street scene appearance in relation to the NPPF, Core Policy 8 of the LDF Core Strategy nor Policy EN1 of the adopted Local Plan.

10.0 **Impact on neighbouring occupiers/Uses**

10.1 Given the site is located within an existing business area with existing industrial buildings adjoining the site on three of its four sides and given that there are no significant windows within the flank walls and notwithstanding that the proposed building would be higher than neighbouring properties, there would no significant impact on neighbouring properties/uses.

10.2 No objections are raised on grounds of impact in relation to the NPPF, Core Policy 8 of the LDF Core Strategy nor Policy EN1 of the adopted Local Plan.

11.0 **Transport, Access Parking and Servicing.**

11.1 With respect to travel patterns and car parking the applicants were advised at the pre application stage that:

"It is unclear as to how much car parking there is on the existing site and this should be made clear in the future planning application."

Under the Slough Local Plan Parking Standards a B2 industrial use in an existing business

area has a parking standard of 1 space per 50sqm. Therefore for site of this size a provision of 58 spaces would be appropriate under the standard. However the proposed site has only 14 spaces (2 of which are for disabled use) and therefore there is a significant shortfall of car parking provision.

I understand that the site operators own at least one other building in this road and which has further parking which could be used by staff from this site. If off site parking is to be used then this needs to be made clear in any future application as well as how many staff are employed in the other building(s).

There is a shortfall of parking spaces on the wider Poyle Trading Estate which leads to a lot of footway parking which causes a hazard and an obstruction to pedestrians and can cause manoeuvring difficulties for goods vehicles seeking to access sites”.

- 11.2 The applicants have submitted a Transportation Technical Note, which provides some but not all of the additional information which was being sought. The report does not provide details as to existing parking levels on site, but confirms the existing floor area of the combined buildings to be 1,228 sq m. The report confirms that 40 existing employees and a two shift pattern 06.00 -14.00 and 14.00 – 22.00.
- 11.3 The transport statement advises that approximately 33% of the current workforce use the car share scheme and a significant proportion arrive using alternative modes such as walking cycling or public transport. It is further estimated that 50% of the existing workforce arrive by car equating to 20 employees split between two shifts.
- 11.4 The proposed building will have a floor area of 3000 sq m requiring 60 no. on site car parking spaces. The actual provision is for only 14 no. spaces. The applicants advise that total number of employees will rise to between 60 and 65, although a three shift system will operate and that the maximum number of employees working at the site at any given time will be between 25 and 30 and that assuming a similar pattern of travel to that which currently exists a total of 15 no. staff would require parking on site at any one time. The applicant advises that 20 no. parking spaces would be a robust figure during the busiest times of operation.
- 11.5 The applicant further advises that the operator also owns two other industrial buildings within Blackthorne Road located approximately 350 metres from the application site. To accommodate the shortfall in on site car parking, the applicant/operator advises that 8 no. car parking spaces will be provided at either or both of these sites. At the time of writing this report, the applicant has not provided information on the current usage of these alternative sites in terms of on site car parking. This information has been requested and the results of which will be reported on the Amendment Sheet.
- 11.6 With respect to alternative modes of travel the following comments were provided at the pre application stage:

In the planning application it would be helpful to understand how many staff work are on-site and by what mode of travel staff usually use to reach the site. A Travel Plan would normally be required for a B2 industrial site of 4000sqm or greater through the planning process, but in some cases for example where there are significant shortfalls of parking then travel plans are requested on smaller sites in order to better manage travel. Therefore there may be a case to request a travel plan for this site. Therefore it would be helpful prior to the submission of the application to further understand some of the issues that are raised in this comment. Taking account of the adjoining building which also forms part of the bakery, although not part of this application per se, if this floor area is added then the size of the overall development may be close to or exceed 4000sqm.

The local highway authority is keen to encourage a greater use of non-car modes of travel

to the Poyle Trading Estate.

- 11.7 An obligation to submit a travel plan will be required under the terms of a S106 Agreement. With respect to parking, this will require more assessment and will be further reported on the Amendment Sheet.

- 11.8 With respect to lorry parking and servicing the following comments were provided at the pre application stage:

Under the Slough Local Plan parking standards, HGV spaces should be provided at 1 space per 500sqm and therefore for this site, 6 HGV bays should be provided. There is space for 4 HGVs to park clear of the carriageway and footway in relation to this site, so again a small shortfall of HGV parking compared to the standard.

The wider Poyle Trading Estate is affected by inconsiderate lorry parking which has been causing anti-social behaviour issues to which the Council has sought to address by implementing three schemes to deter overnight lorry parking and lorry parking on the public highway. Whilst the schemes have been successful in their immediate areas at deterring the problem it has in part pushed the problem to another area in the estate.

The submitted site plan demonstrates using autotracking that 10m long rigid HGVs can reverse into the site and leave in a forward gear, whilst it would be preferred if vehicles were enter and leave in a forward gear this is not possible taking into account the constraints of the site. The proposed servicing arrangement is considered acceptable.

- 11.9 The Transport Statement advises that the proposal is for all deliveries to be received by the company's sister premises on Blackthorne Road, where the loads will be broken down and then decanted to the application site using smaller delivery vehicles. The deliveries to the David Road site will be one delivery per day and would be scheduled outside of peak hours to further minimise disruption within the industrial estate.

- 11.10 Subject to the acceptance of these proposals by the Council's transport and highway engineers, a S106 Agreement will be required such that there are adequate links and safeguards in place to ensure that any provision for off site parking and servicing remains in place, whilst the proposed operator continues to operate from the proposal property. Given the shortfall in car and HGV parking, it will also be necessary to ensure that the planning permission be made personal to the occupier.

- 11.11 At the pre application stage advice was provided on HGV lorry movements:

The local highway authority has been made aware by local residents and councilors of concerns about the increase in HGV traffic accessing the Poyle Trading Estate by using the A4 Old Bath Road rather than accessing via M25 junction 14. The reason for this concern is the increase in noise, disturbance and other environmental impacts of HGV traffic on the residential area. The local highway authority is considering introducing a scheme to deter HGV access from Old Bath Road in the future and therefore it is appropriate to encourage existing users of the Estate to route HGV traffic from junction 14 or from the west via Stanwell Road.

- 11.12 Subject to further confirmation from the Council transport and highway engineers, a lorry routing plan will be required as part of a S106 Agreement.

- 11.13 With respect to access at the pre application stage the following advice was given:

The application seeks to provide three accesses to the development, two of which are proposed for HGV use and one for staff vehicles. The two accesses proposed for the HGVs should be provided as bell mouths and the access for cars should be provided in the

form of a vehicle crossover. As a result of these changes there will need to be changes to the existing access points and where appropriate new access points created and redundant access points removed and the footway reinstated. Taking account of the proposed changes this is likely to require the footway to be reconstructed along the frontage of the site, with relocated gullies and lamp columns where appropriate. The applicant would need to enter into a Minor Highways Works agreement to undertake the works or alternatively use the Council's term contractor.

11.14 These matters would be covered by appropriate planning conditions.

12.0 **Flood Risk Drainage and Land Contamination**

12.1 The site is not located within either flood zones 2 or 3, nor is it located within an identified critical flood risk area within flood zone 1. In terms of managing surface water disposal, this will necessitate on site mitigation/storage to achieve an acceptable run off rate. Planning conditions will be imposed requiring details of drainage to be submitted for approval.

12.2 The Land Contamination Officer advises a precautionary condition will be required in terms of a Desk Top Land Contamination Study with mitigation and validation if contamination is identified and this will be covered by planning condition.

13.0 **Section 106**

13.1 At the pre application stage the transport and highway engineers recommended a number of potential mitigation measures:

Taking account that there will be an increase in floor area and therefore most likely an increase in vehicle trips to the site. There is the potential for requesting mitigation measures, which may include but would not be limited to implementing a Travel Plan. The mitigation measures would be secured through a S106 agreement and it is most likely that they would be in the form of a financial contribution towards some of the following measures:

- To improve facilities for pedestrian, cycle and public transport trips to and from the Poyle Trading Estate;
- To discourage HGV parking on the public highway on the Poyle Trading Estate; and
- To discourage hazardous car parking on the public highway on the Poyle Trading Estate.

However, these mitigation measures will need to be firmed up to be more site specific in light of restrictions to the pooling of S106 financial contributions. Further discussions with the transport and highway engineers will be undertaken and any further recommendations will be reported on the Amendment Sheet.

13.2 In addition to the above the following draft Heads of Terms for a S106 Agreement are proposed:

- Submission of a travel plan
- Payment of a travel plan monitoring fee
- HGV routing agreement
- The submission of a servicing and deliveries plan. As part of that plan, all HGV deliveries shall be made at one or both of its sister companies in Blackthorne Road, where the loads shall be broken down and decanted to the application site using smaller vehicles. Such deliveries to take place outside of the peak hours.
- The submission of a car parking management plan, which shall include a requirement on the owner/developer to make available on one or both of its sister companies in Blackthorne Road a minimum of 8 no. car parking spaces for use by staff working at the application site. In the event that either or both of the sister companies are sold and the additional car parking spaces can no longer be

provided, that alternative car parking provision be provided off site, within a time frame to be agreed. If this is not achieved within the timescales agreed that the use of the proposal property is to cease until the issue has been satisfactorily resolved.

- Notify the Council if either or both of the sister company sites in Blackthorne Road are to be sold

PART C: RECOMMENDATION

14.0 Recommendation

- 14.1 Delegate the planning application to the Planning Manager for resolution of outstanding highway issues, consideration of outstanding consultations, any minor design changes, finalising conditions completion of a S106 Agreement and final determination.

PART D: LIST OF CONDITIONS AND INFORMATIVES

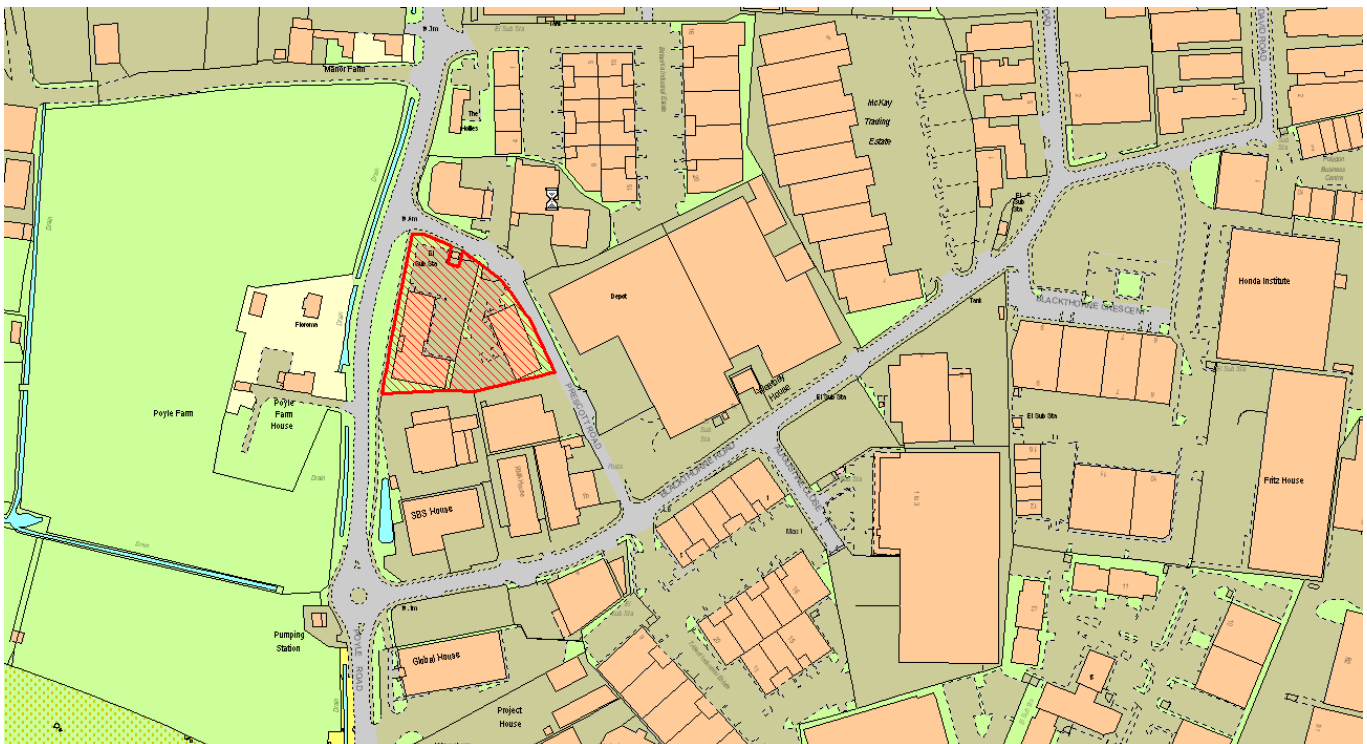
15.0 Conditions

- 15.1 The following summary of conditions is proposed:

- Time
- Personal Planning Permission
- Approved Drawings
- Samples of Materials
- Surface Materials
- Landscape & Boundary Treatment
- Minimum Car Parking
- Details of Bin Store & Secure Cycle Parking
- Pedestrian Visibility
- Waste Minimisation Plan
- Construction Management Plan Drainage Strategy Car Parking Management Plan (unless covered in S106 Agreement)
- Servicing and Deliveries Plan (unless covered in S106 Agreement)
- Land Contamination Study
- Hours of Construction
- Hours of Deliveries
- Vehicle Access Gates
- Reinstate Redundant Crossovers.
- Details of Access
- Maximum number of employees

| | | | |
|--------------------|--|--------------|----------------------|
| Registration Date: | 11-Oct-2013 | Applic. No: | P/14486/001 |
| Officer: | Ian Hann | Ward: | Colnbrook with Poyle |
| | | Applic type: | 13 week |
| | | date: | |
| Applicant: | Mr. Derek Price, DWP Associates | | |
| Agent: | DWP Associates 1, Glanmorfa, Ferryside, SA17 5TF | | |
| Location: | Unit 1, Prescott Road, Colnbrook, Slough, SL3 0AE | | |
| Proposal: | ERECTION OF 2789 SQUARE METRE WAREHOUSE BUILDING WITH ANCILLARY OFFICE SPACE AND ASSOCIATED PARKING FOLLOWING DEMOLITION OF EXISTING BUILDINGS | | |

Recommendation: Delegate to Planning Manager



1.0 **SUPPLEMENTARY REPORT**

1.1 This application was previously before committee on 9th January 2014 when the application was delegated to the then Strategic Lead Planning Policy for the completion of a S106 Agreement, consideration of a tree report, finalising conditions and final determination. A copy of the previous report is attached at appendix "A" and the amendment sheet at appendix "B". Since then there has been a delay in signing the Section 106 Agreement as a section of land within the application boundary is unregistered, and as a result of this part of the visibility splay can not be transferred to the Highways Authority as requested under the Section 106 Agreement. This matter is currently being discussed between the Council's legal officers and the applicant's Solicitors and the matter should be able to be resolved.

1.2 This is a full planning application for the redevelopment of the application site to provide 2,500 sq. metres of storage and distribution (B8) use including ancillary office space (306.5 sq. metres) together with associated car parking, servicing and landscaping following the demolition of the existing building.

1.3 The applicant's have now submitted amended plans seeking the following changes from the plans that were previously before committee:

- moving the loading doors closer together
- removing the previously proposed parapet roof and replacing it with a pitched roof increasing the height from 10m to 11.6m.
- amendments to the elevation cladding material with grey and silver cladding being used
- alterations to the office elevations with an increase in the number of windows and type of windows including the relocation of the main building entrance.
- A change in car parking spaces from 14 to 13.

2.0 **Recommendation**

Committee is asked to endorse the changes to the original scheme and reconfirm that the application be delegated the planning application to Planning Manager for consideration of tree report, finalising conditions, completion of Section 106 Agreement and final determination.

1.0 **SUMMARY OF RECOMMENDATION**

- 1.1 Delegate the planning application to Strategic Lead Planning Policy for the consideration of any transport and highways issues, finalising conditions and final determination.
- 1.2 This application is to be decided at Planning Committee as it is a major development.

PART A: BACKGROUND

2.0 **Proposal**

- 2.1 This is a full planning application for the redevelopment of the application site to provide 2,500 sq. metres of storage and distribution (B8) use including ancillary office space on a mezzanine floor (306.5 sq. metres) together with associated car parking, servicing and landscaping following the demolition of the existing building.

The application is accompanied by plans showing the site location, site layout, elevations and floor plans. The following is also submitted:

- Planning, Design and Access Statement
- Transport Statement
- Flood Risk Assessment

- 2.2 The plans that has been submitted shows a proposed building that will have a footprint of approximately 39m by 58m and with a height of approximately 9.6m.

- 2.3 The plans show that the building would comprise a ground floor warehouse and ancillary offices on a mezzanine floor. The building would be finished in metal cladding and windows in the northern elevation facing onto Prescott Road that will serve the offices. Two lorry loading bays with roller shutter doors will be provided on the side elevation facing on to Prescott Road.

- 2.4 The development would be accessed by the existing access from Prescott Road. 14 car parking spaces would be provided with spaces for lorries that are not being unloaded.

- 2.5 The whole site will be surrounded by security fencing with a sliding gate onto Blackthorne Road and additional planting onto Blackthorne Road also.

3.0 **Application Site**

- 3.1 The application site is situated on the south east of the junction of Poyle Road and Prescott Road and is an Existing Business Area as identified in the adopted Local Plan.

- 3.2 The site has an area of approximately 00.49 hectares and is roughly triangular, measuring approximately 87m wide and 80m deep.

The site is currently occupied by two vacant light industrial buildings.

The site is bound by Poyle Road to the west with farm land beyond, Prescott Road to the east with industrial buildings with further industrial buildings to the north and south.

The surrounding buildings are mostly warehouses and industrial buildings.

4.0 **Relevant Site History**

- 4.1 Planning permission was granted for the conversion of the existing two storey unit into 5no. Two storey units incorporating change of use to B1/B8 units with the addition of external balcony as a means of escape and roller shutter doors with other external alterations in January 2009 (P/14486/000).

5.0 **Neighbour Notification**

- 5.1 Warren Insulation Plc, Sbs House 1, Blackthorne Road, Colnbrook

1, 2, 3, 4, Poyle Road, Colnbrook

Unit 1, 1a Prescott Road, Colnbrook

No comments have been received to date, any comments will be reported on the Committee Amendment Sheet.

5.2 **Colnbrook with Poyle Parish Council**

Consulted although no comments received to date. If comments are received these will be reported on in the Amendment Sheet.

6.0 **Consultation**

6.1 **Transport and Highways**

Consulted although no comments received to date. If comments are received these will be reported on in the Amendment Sheet.

6.2 **Environment Agency**

This application is deemed to either have a low environmental risk or relate to conditions that were not recommend by the Environment Agency who are unable to make an individual response at this time.

6.3 **Neighbourhood Protection / Environmental Health**

There are no objections to the development but the scale of project would suggest that a Site Waste Management Plan would be appropriate together with a ban on any burning of waste on site.

6.4 **BAA Safeguarding**

Consulted although no comments received to date. If comments are received these will be reported on in the Amendment Sheet.

PART B: PLANNING APPRAISAL

7.0 **Policy Background**

- 7.1 The application is considered alongside the following policies:

National guidance

- National Planning Policy Framework and technical guidance notes.

Local Development Framework, Core Strategy 2006-2026, Development Plan Document

- Core Policy 1 (Spatial Vision and Strategic Objectives for Slough)
- Core Policy 5 (Employment)
- Core Policy 6 (Retail, Leisure and Community Facilities)
- Core Policy 7 (Transport)
- Core Policy 8 (Sustainability & the Environment)
- Core Policy 10 (Infrastructure)

Adopted Local Plan for Slough

- EN1 (Standard of Design)
- EN3 (Landscaping Requirements)
- EN5 (Design and Crime Prevention)
- EMP2 (Criteria for Business Developments)
- EMP9 (Lakeside Road Estate, Galleymead Road and the Poyle Estate)
- T2 (Parking Restraint)

7.2 The main planning considerations are therefore considered to be:

- Principle of the redevelopment & land use
- Design and appearance
- Impact on adjoining sites
- Traffic and Highways Implications

Assessment

8.0 **Principle of the redevelopment & land use**

8.1 The NPPF states that unless material considerations dictate otherwise development proposals that accord with the development plan should be approved without delay. That planning should not act as an impediment to sustainable growth and should avoid the long term protection of sites allocated for employment use where there is no reasonable prospect of a site being used for that purpose. It also states that high quality design should be secured and a good standard of amenity for all existing and future occupants of land and buildings. Permission should be refused for development of poor design that fails to take the opportunities available for improving the character and quality of an area and the way it functions.

8.2 Policy EMP2 (Criteria for Business Developments) states:
"Proposals for business developments will only be permitted if they comply with all of the following criteria:
a) the proposed building is of a high quality design and is of a use and scale that is appropriate to its location;
b) it does not significantly harm the physical or visual character of the surrounding area and there is no significant loss of amenities for the neighbouring land uses as a result of noise, the level of activity, over- looking, or overbearing appearance of the new building;
c) the proposed development can be accommodated upon the existing highway network without causing additional congestion or creating a road safety problem;
d) appropriate servicing and lorry parking is provided within the site;
e) appropriate contributions are made to the implementation of any off-site highway works that are required and towards other transport improvements such as pedestrian and cycle facilities, that are needed in order to maintain accessibility to the development without increasing traffic congestion in the vicinity or in the transport corridors serving the site;
f) the proposal incorporates an appropriate landscaping scheme;
g) the proposal would not significantly reduce the variety and range of business premises;"

8.3 Policy EMP9 (Lakeside Road Estate, Galleymead Road and the Poyle Estate) states that *“B1(b) research and development, B1(c) light industrial, B2 general industrial and B8 storage and distribution will be permitted within the Lakeside Road Estate, Galleymead Road and the Poyle Estate. Additional independent B1(a) office floorspace will not be permitted in this location.”*

8.4 The principle for the type of redevelopment is considered to be acceptable within an existing Business Area where the type of use proposed is acceptable and compatible with other surrounding uses.

8.5 No objections are raised to the principle of constructing a Class B8 Storage and Distribution Warehouse on the application site in relation to the National Planning Policy Framework, Core Policy 5 or Local Plan Policies EMP5 and EMP9.

9.0 **Design and Appearance**

9.1 The National Planning Policy Framework states that *“great importance to the design of the built environment. Good design is a key aspect of sustainable development, is indivisible from good planning, and should contribute positively to making places better for people.”*

Policy EN1 of the adopted Local Plan states that development proposals are required to reflect a high standard of design and must be compatible with and/ or improve their surroundings in terms of scale, height, massing/ bulk, layout, siting, building form and design, architectural style, materials, access points and servicing, visual impact, relationship to nearby properties, relationship to mature trees; and relationship to watercourses.

Core Policy 8 of the Core Strategy requires that, in terms of design, all development:

- a) Be of high quality design that is practical, attractive, safe, accessible and adaptable;
- b) Respect its location and surroundings;
- c) Provide appropriate public space, amenity space and landscaping as an integral part of the design; and
- d) Be in accordance with the Spatial Strategy in terms of its height, scale, massing and architectural style.

9.2 The footprint of the building is considered to be consistent with the size of other large industrial buildings found elsewhere within the surrounding area and estates. The height of the building would be higher than the adjacent premises. However the site would be in keeping with other industrial properties and the site is considered to be a large enough site to support a building of slightly larger bulk and mass. The development would be set back a minimum of 11 metres from the Poyle Road frontage where the existing mature tree screen will be retained to help soften and break up the scale of the development.

9.3 The plans show that the building development would comprise ground floor warehouse with ancillary offices at a mezzanine floor level which would be oriented to the northern side of the site facing Prescott Road. The layout is considered to be logical and would maximise efficiency. The offices will also be served with windows that will form the elevation of the building and therefore break up the façade of the building facing onto Prescott Road.

9.4 Full details of the elevations and appearance of the building have been provided. The architectural style proposed for the development uses clean, simple lines and is modern. This fits in with the style and appearance of many of the buildings, especially those warehouse type buildings within the local area. The building would be finished in metallic silver cladding which will be broken by roller shutter doors in the eastern elevation. Although this could be considered to be bland in appearance it is in keeping with the industrial nature of the area and other buildings within the industrial area have similar

appearances. The elevation facing onto Prescott Road will be broken up with windows that will provide a more interesting façade onto Prescott Road, in keeping with its surroundings and have no detrimental impact upon the character of the area. Overall the design and appearance of the development is considered to be in keeping with other modern industrial buildings found elsewhere within the area with the offices providing a high quality frontage to Prescott Road and that this would improve the appearance of the site.

9.5 It is proposed to fence all the boundaries of the site with a Security Fence, full details of which can be secured via condition. While a security fence could be considered to be overbearing it will allow views into the site and with substantial landscaping around the frontage facing onto Poyle Road that will be seen from the public road, it is considered that the screening will take away the harshness of the fencing and that it will not have any detrimental impact upon the character of the area.

9.6 The design and appearance of the development is considered to be consistent with the relevant policies and government guidance.

10.0 **Impact on adjoining sites**

10.1 Policy EMP2 of the Local Plan requires that: *“there is no significant loss of amenities for the neighbouring land uses as a result of noise, the level of activity, overlooking, or overbearing appearance of the new building”*.

Core Policy 8 states *“Development shall not give rise to unacceptable levels of pollution including air pollution, dust, odour, artificial lighting or noise”*.

10.2 The proposed layout of the site would bring the development closer to the northern and southern boundaries than the previous building which is to be demolished. The nearest building to the site would be 10m away and there would be no impact up on the working conditions of that building or any other surrounding building. There are no nearby residential properties that will be effected by the proposals.

10.3 In terms of environmental effects, no air conditioning or plant, have been indicated on the submitted plans. A condition can be attached to any permission to require that no machinery, plant, ducts or other openings be allowed without the prior written approval of the Local Planning Authority. In terms of lighting, the Design and Access Statement indicates that the external lighting shall be designed to comply with legislation on light pollution and Heathrow approach restrictions. Again a standard lighting condition can be attached to any permission to secure adequate lighting around the site.

10.4 The proposal is considered to be in accordance with Core Policy 8 and policy EMP2 of the adopted Local Plan.

11.0 **Traffic and Highways Implications**

11.1 Core Policy 7 (Transport) of the Slough Local Development Framework, Core Strategy 2006-2026, (Submission Document), requires that: *“All new development should reinforce the principles of the transport strategy as set out in the Council’s Local Transport Plan and Spatial Strategy, which seek to ensure that new development is sustainable and is located in the most accessible locations, thereby reducing the need to travel.*

Development proposals will, either individually or collectively, have to make appropriate provisions for:

- *Reducing the need to travel;*
- *Widening travel choices and making travel by sustainable means of transport more attractive than the private car;*
- *Improving road safety; and*

- *Improving air quality and reducing the impact of travel upon the environment, in particular climate change.*

There will be no overall increase in the number of parking spaces permitted within commercial redevelopment schemes unless this is required for local road safety or operational reasons.”

The supporting text to Policy EMP9 (Poyle Estate) notes that *“on the Poyle Estate, provision for parking and servicing arrangements is limited, and in many cases does not meet current standards, resulting in congestion on the estate. Redevelopments will be expected to improve vehicular access and overcome road safety problems.”* It acknowledges that there is very limited public transport provision, and therefore access to this area is mainly by car for the workforce and visitors, and goes on to say *“The Borough Council will continue to encourage the location of B8 distribution/storage and freight activity within these three areas, and B1(b) research and development, B1(c) light industrial activity, and B2 general industrial would also be acceptable. As parking provision will be in accordance with Appendix 2, an increase in current parking provision may be required to overcome localised operational or road safety problems.”*

Policy EMP2 (Criteria for Business Developments) of the Local Plan states that: *“Proposals for business developments will only be permitted if they comply with all of the following criteria:*

- c) the proposed development can be accommodated upon the existing highway network without causing additional congestion or creating a road safety problem;*
- d) appropriate servicing and lorry parking is provided within the site;*
- e) appropriate contributions are made to the implementation of any off-site highway works that are required and towards other transport improvements such as pedestrian and cycle facilities, that are needed in order to maintain accessibility to the development without increasing traffic congestion in the vicinity or in the transport corridors serving the site”.*

11.2 It is proposed that the development would provide 14 car parking spaces. To this end, the proposal is consistent with Council’s policy of *no overall increase in the number of parking spaces permitted within commercial redevelopment schemes* (Core Policy 7) while still complying with the Council’s adopted parking standards.

11.3 With the existing access being used and with the footprint of the building being smaller than the existing building it is considered that the proposals will have no adverse impact upon highway safety and will not result in a unacceptable increase in the number of trips although the Council’s Transport and Highways Engineers are still to confirm that it meets their requirements. A condition will need to be added to any permission to ensure that the gates are open when the building is in use so as to avoid vehicles waiting on the highway.

12.0 **Summary**

12.1 On the basis of the information provided it is considered that the proposals would not have a detrimental impact upon the character of the area or neighbouring amenity and the application should be approved subject to conditions.

PART C: RECOMMENDATION

13.0 **Recommendation**

Delegate the planning application to Strategic Lead Planning Policy for the consideration of any transport and highways issues, finalising conditions and final determination.

14.0 **PART D: LIST OF CONDITIONS AND INFORMATIVES**

Please note that this is not the final list of conditions and amendments may be made prior to planning permission being granted.

14.1 **CONDITIONS**

1 Time limit

The development hereby permitted shall be commenced within three years from the date of this permission.

REASON To prevent the accumulation of planning permissions, and to enable the Council to review the suitability of the development in the light of altered circumstances and to comply with the provisions of Section 91 of the Town and Country Planning Act 1990.

2 Approved Plan

The development hereby approved shall be implemented only in accordance with the following plans and drawings hereby approved by the Local Planning Authority:

- (a) Drawing No PA/02 Dated Undated Recd On 10/10/2013
- (b) Drawing No PA/07 Dated Undated Recd On 10/10/2013
- (c) Drawing No PA/03 Dated Undated Recd On 10/10/2013
- (d) Drawing No PA/04 (elevations) Dated Undated Recd On 10/10/2013
- (e) Drawing No PL-012 Dated Undated Recd On 25/10/2013
- (f) Drawing No PL-013 Dated Undated Recd On 25/10/2013
- (g) Drawing No PL-014 Dated Undated Recd On 25/10/2013

REASON To ensure that the site is developed in accordance with the submitted application and to ensure that the proposed development does not prejudice the amenity of the area and to comply with the policies in The Local Plan for Slough 2004.

3 Details of external materials

Samples of external materials to be used on the development hereby approved shall be submitted to and approved in writing by the Local Planning Authority before the scheme is commenced on site and the development shall be carried out in accordance with the details approved.

REASON To ensure a satisfactory appearance of the development so as not to prejudice the visual amenity of the locality in accordance with Planning Policy Statement 1: Delivering Sustainable Development (2005), Core Policy 8 of the Adopted Local Development Framework Core Strategy 2006-2026 (Development Plan Document, Dec 2008) and Policy EN1 of the Adopted Local Plan for Slough, 2004.

4 Details of surfaces

Samples of external materials to be used in the construction of the access, parking, circulation, pathways and communal areas of each phase within the development hereby approved shall be submitted to and approved in writing by the Local Planning Authority before that phase of the development is commenced on site and the development shall be carried out in accordance with the details approved.

REASON To ensure a satisfactory appearance of the development so as not to prejudice the visual amenity of the locality in accordance with Planning Policy Statement 1: Delivering Sustainable Development (2005), Core Policy 8 of the Adopted Local Development Framework Core Strategy 2006-2026 (Development Plan Document, Dec 2008) and Policy EN1 of the Adopted Local Plan for Slough, 2004.

5 Maximum floor space and removal of PD rights

Notwithstanding the provisions of the Town and Country Planning General Permitted Development Order 1995 (or any order revoking and re-enacting that Order) the total gross floor space of the building hereby permitted shall not exceed 2,500 sq. metres and no extension or alteration either external or internal, involving an increase in floor space above the approved 2,500 sqm including a mezzanine floor, shall be carried out without the prior permission of the Local Planning Authority.

REASON To retain control over the intensification of the use of the site, particularly having regard to the provision of onsite parking.

6 Limit on ancillary office space

Ancillary office space (excluding service cores) shall not cover more than 307 sq metres without the prior consent of the Local Planning Authority.

REASON To control the amount of office development on the site in the interests of sustainability and to accord with Core Policy 5 of the Local Development Framework, Core Strategy 2006-2026 (Development Plan Document, Dec 2008) and Policy EMP9 of the Adopted Local Plan for Slough, 2004.

7 Car parking

The parking spaces and turning area shown on the approved plan shall be provided on site prior to occupation of the development and retained at all times in the future for the parking of motor vehicles.

REASON To ensure that adequate on-site parking provision is available to serve the development and to protect the amenities of the area in accordance with Policy T3 of The Local Plan for Slough 2004.

8 Details of gates

No development shall be occupied until details of the vehicle access gates have been agreed in writing with the Local Planning Authority and shall remain open during the operational hours of the building.

REASON To enable service vehicles to draw off the highway to minimise danger, obstruction and inconvenience to users of the adjoining highway in accordance with Planning Policy Guidance 13: Transport (2001), Core Policy 7 of the Adopted Local Development Framework Core Strategy 2006-2026 (Development Plan Document, Dec 2008).

9 Cycle parking

No development shall be begun until details of the cycle parking provision of that phase (including location, housing and cycle stand details) have been submitted to and approved in writing by the Local Planning Authority. The cycle parking shall be provided in accordance with these details prior to the occupation of that phase of the development and shall be retained for so long as the development continues to be used for the purposes authorised by this permission.

REASON To ensure that there is adequate cycle parking available at the site in accordance with Planning Policy Guidance 13: Transport (2001), Core Policy 7 of the Adopted Local Development Framework Core Strategy 2006-2026 (Development Plan Document, Dec 2008) and Policy T8 of the Adopted Local Plan for Slough, 2004, and to meet the objectives of the Slough Integrated Transport Strategy.

10 Flood risk

The development permitted by this planning permission shall only be carried out in accordance with the approved Flood Risk Assessment LS1366/FRA001 by BSCP dated 27th September 2013.

Reason

To reduce the risk of flooding to the proposed development and future occupants.

11 Boundary treatment

No development shall commence on site until details of the proposed boundary treatment including position, external appearance, height and materials have been submitted to and approved by the Local Planning Authority. The boundary treatment shall be implemented in accordance with the details approved prior to the first occupation of the development and retained thereafter.

REASON In the interests of the visual amenity of the area and accordance with Policies EN1 and EN3 of the Adopted Local Plan for Slough, 2004.

12 Details of plant and machinery

The use of each phase of development hereby permitted shall not commence until details of the external plant (including siting) to be installed at the site for that phase have been submitted to and approved in writing by The Local Planning Authority. The plant shall be installed in accordance with the approved details prior to first occupation of that phase of the development.

REASON To protect the amenities of the area in accordance with Core Policy 8 of the Slough Local Development Framework, Core Strategy 2006 - 2026, Development Plan Document, December 2008.

13 Plant and machinery acoustic treatment

All air conditioning, ventilation or other plant and machinery shall be designed to ensure that external noise generated by the plant or equipment shall not at any time exceed the ambient sound level as measured at the site boundary when the equipment is not in operation. This shall be implemented prior to first occupation of the development and retained at all times in the future.

REASON To minimise the impact of the noise generated by the equipment on the amenities of the local residents in accordance with Core Policy 8 of the Adopted Local Development Framework, Core Strategy 2006 – 2026 (Development Plan Document, December 2008).

14 Construction management

Prior to the commencement of development a construction management plan and programme shall be submitted to and approved in writing by the Local Planning Authority. The construction management plan and programme shall include details of the following:

- Details of contractor parking available
- A strategy for the management of construction traffic to and from the site together with details of parking/ waiting for demolition/ construction site staff and for delivery vehicles

The details as approved shall be fully implemented at all times for the duration of demolition and construction works.

REASON So as not to prejudice the free flow of traffic along the neighbouring highway and in the interests of highway safety in accordance with Core Policy 7 of the Adopted Local Development Framework, Core Strategy 2006 – 2026 (Development Plan Document, December 2008).

15 Control of environmental effects

No development shall begin until details of a scheme (Working Method Statement) to control the environmental effects of construction work has been submitted to and approved in writing by the Local Planning Authority. The scheme shall include:

- (i) control of noise
- (ii) control of dust, smell and other effluvia
- (iii) control of surface water run off
- (iv) site security arrangements including hoardings
- (v) proposed method of piling for foundations
- (vi) construction working hours, hours during the construction phase when delivery vehicles taking materials are allowed to enter or leave the site.

The development shall be carried out in accordance with the approved scheme or as otherwise be agreed in writing by the Local Planning Authority.

REASON In the interests of the amenities of the area in accordance with Core Policy 8 of the Adopted Local Development Framework Core Strategy 2006-2026 (Development Plan Document, Dec 2008).

16 Hours of demolition and construction

No construction work shall take place outside the hours of 08:00 - 18:00 hrs Monday to Friday, 08:00 - 13:00 hrs on a Saturday and no working at all on Sundays or public holidays or as otherwise may be agreed in writing by the Local Planning Authority

REASON In the interests of the amenities of the area in accordance with Core Policy 8 of the Adopted Local Development Framework Core Strategy 2006-2026 (Development Plan Document, Dec 2008).

17 Control of waste during construction phase

No development of each phase shall take place until details in respect of measures to control the disposal of waste generated during the construction and the use of the development of that phase have been submitted to and approved in writing by the Local Planning Authority. The approved details shall be implemented during the course of building operations and the subsequent use of the building:

- (a) Minimise, re-use and re-cycle waste, including materials and waste arising from construction;
- (b) Minimise the pollution potential of unavoidable waste;
- (c) Dispose of unavoidable waste in an environmentally acceptable manner – there shall be no bonfires on site.

REASON In the interests of the amenities of the area in accordance with Core Policy 8 of the Adopted Local Development Framework Core Strategy 2006-2026 (Development Plan Document, Dec 2008).

18 On-site refuse storage

No development shall take place until details of on-site storage (including any open air storage facilities) for waste material awaiting disposal (including details of any screening) have been submitted to and approved in writing by the Local Planning Authority. Such facilities shall be provided in accordance with the approved details prior to the first occupation of that phase of the development and thereafter retained for so long as the development continues to be used for the purposes authorised by this permission.

REASON In the interests of the amenities of the area in accordance with Core Policy 8 of the Adopted Local Development Framework Core Strategy 2006-2026 (Development Plan Document, Dec 2008).

INFORMATIVES

1. In dealing with this application, the Local Planning Authority has worked with the applicant in a positive and proactive manner through pre-application discussions. It is the view of the Local Planning Authority that the proposed development does improve the economic, social and environmental conditions of the area for the reasons given in this notice and it is in accordance with the National Planning Policy Framework.

Amendment sheet

Comments have been received from British Airports Authority Environment Agency who raise no objections to the planning application and have recommended the following conditions and informatives which will be incorporated into the list of final conditions:

CONDITION

1. Development shall not commence until a Bird Hazard Management Plan has been submitted to and approved in writing by the Local Planning Authority. The submitted plan shall include details of:
 - *Management of any flat/shallow pitched/green roofs on buildings within the site which may be attractive to nesting, roosting and "loafing" birds. The management plan shall comply with Advice Note 8 'Potential Bird Hazards from Building Design' attached*

The Bird Hazard Management Plan shall be implemented as approved and shall remain in force for the life of the building. No subsequent alterations to the plan are to take place unless first submitted to and approved in writing by the Local Planning Authority.

Reason: It is necessary to manage the flat/green roof in order to minimise its attractiveness to birds which could endanger the safe movement of aircraft and the operation of Heathrow Airport.

INFORMATIVES

1. The Bird Hazard Management Plan must ensure that flat/shallow pitched roofs be constructed to allow access to all areas by foot using permanent fixed access stairs ladders or similar. The owner/occupier must not allow gulls, to nest, roost or loaf on the building. Checks must be made weekly or sooner if bird activity dictates, during the breeding season. Outside of the breeding season gull activity must be monitored and the roof checked regularly to ensure that gulls do not utilise the roof. Any gulls found nesting, roosting or loafing must be dispersed by the owner/occupier when detected or when requested by BAA Airside Operations staff. In some instances it may be necessary to contact BAA Airside Operations staff before bird dispersal takes place. The owner/occupier must remove any nests or eggs found on the roof.
2. The breeding season for gulls typically runs from March to June. The owner/occupier must obtain the appropriate licences where applicable from Natural England before the removal of nests and eggs.
3. The development is close to the aerodrome and the approach to the runway. We draw attention to the need to carefully design lighting proposals. This is further explained in Advice Note 2, 'Lighting near Aerodromes' (available at [http://www.aoa.org.uk/operation & safety/safeguarding.htm](http://www.aoa.org.uk/operation%20&%20safety/safeguarding.htm)). Please note that the Air Navigation Order 2005, Article 135 grants the Civil Aviation Authority power to serve notice to extinguish or screen lighting which may endanger aircraft.
4. Given the nature of the proposed development it is possible that a crane may be required during its construction. We would, therefore, draw the applicant's attention to the requirement within the British Standard Code of Practice for the safe use of Cranes, for crane operators to consult the aerodrome before erecting a crane in close proximity to an aerodrome. This is explained further in Advice Note

The following comments have been received from the Council's Transport Engineer:

Trip Generation

The consultant has derived trip generation information from the TRICS database to inform the assessment and the number of sites used is considered to be rather low. The TS reports that the development will generate 186 vehicle movements per day compared to its existing use as an office generating 118 vehicle movements. Therefore there will be an increase of 68 movements per day. The sites in TRICS used by the applicant do not include 24 hour operation which. It is unclear from the information submitted how many of the vehicle movements will be HGVs, but clearly there will be a greater proportion of HGVs than the current site if it was in operation.

Access

It is proposed to stop up one of the existing accesses to the site, but significantly enlarge the second access. A detailed drawing will need to be supplied of the amendments to the access.

No visibility splays have been shown for the revised access. As the junction is off a "lightly trafficked road" sight lines of 2.4m x 25m must be provided from the priority road onto the site. These should be shown on the drawing and take into account the curvature on the priority road. No obstructions over 600mm in height will be permitted in the sight line areas. The sight lines must fall on land in control of the applicant.

Parking

14 car parking spaces are proposed which is in accordance with the standards in the Slough Local Plan. 5 HGV parking spaces are required under the standards and the plans would appear to indicate that this is achievable, although the location of the roller shutter doors need to be shown on the site plan as it would appear that one of the HGVs is shown parked across it rather than in front of it. The plans should be revised and re-submitted. In the vicinity of the site there are problems with inconsiderate lorry parking on the highway causing highway safety and public amenity issues and therefore ensuring that there is adequate parking and limiting the opportunities for overspill parking or vehicle waiting on the highway for space to become available in delivery yards is an issue that the Council is committed to addressing.

A cycle store is proposed, but it does not appear to be possible to access from within the site. Therefore changes are required to the site plan prior to determination. In terms of security of the cycle store, it should only be accessible from the site and not directly from Prescott Road.

Layout

HGV tracking was requested in the pre-application scope comments, but no tracking has been provided - this needs to be submitted to show that HGVs can adequately access the site and turn within the site.

There would appear to be no direct footway access into the site as the footway on Prescott Road would appear to terminate prior to the pedestrian link into the site. Further plans are required to show that a continuous pedestrian link can be provided into the site. When the site was operating and potentially with the operation of adjoining sites there has been a habit of footway parking along the Prescott Road frontage of the development and therefore I think it is appropriate that the developer funds the £3k cost of a Traffic Regulation Order to implement double yellow lines on the section of Prescott Road between the site access and Poyle Road - this would be beneficial to the operation of the site to assist HGV access to and from Poyle Road.

Accessibility

The TS states that the site benefits from very good accessibility for pedestrians, cyclists and public transport users. I would disagree with this finding. Public transport services along Poyle Road are 1 service an hour in each direction between Slough town centre and Heathrow Terminal 5 via Poyle Road - this does not constitute good accessibility.

Whilst the site is located within cycling distance of Slough and other local settlements, the routes to these settlements have high traffic volumes and high numbers of HGVs and therefore only very few people are willing to make these journeys by cycle.

In terms of pedestrian movement the immediate vicinity of the site is very unattractive for pedestrian movement as there is a very overgrown vegetation strip between the site, the footway and Poyle Road which is a real disincentive for pedestrians to use given its current condition. On my site visit I witnessed female pedestrians in the early evening walking along Poyle Road and its verge to avoid walking along this section of footway. Prompted by the pre-application inquiry in 2012 in relation to this site I sought a price from the Council's grounds maintenance contractor to cut back the vegetation from the road side edge of the path removing stumps, trees, shrubs and ivy and level the area in order to reseed and be able to mow it. To lift all the trees to a height of 4m over the area so that the path has a less enclosed feel. The cost of this works was estimated to be £2,500. I would recommend that this is secured as part of the S106 agreement in order to encourage pedestrian movement to the site.

Gates

Gates will not be permitted on the highway boundary. Gates if required must be set back a minimum distance of 18 metres (articulated vehicles) from the highway boundary and either slide or open inwards. This will allow vehicles including a service vehicle to wait clear of the highway, prior to the gates opening. The current location of the gates has been designed for private car traffic and is not appropriate for the change of use of the site. If the applicant is unwilling to set the gates back then they will need to agree to a condition that the gates shall be secured in an open position during any hours in which the development is in operation.

Mitigation

Whilst the Transport Statement demonstrated that the traffic generated by this proposal can be accommodated at the site access, the development will nevertheless generate additional traffic movements onto the already heavily congested network within the Borough. There is also the potential for inconsiderate lorry parking in the Poyle area, which is an existing and increasing problem in the vicinity of this site.

In recognising that the highway network within the Borough experiences extensive problems with capacity and delay, the Borough Council has developed a Transport Strategy which is supported by central government policy to encourage modal shift to other forms of transport and manage congestion to enable targets within the Transport Act to be met. This development would place additional demands on the transport network on a daily basis and the associated traffic movements would exacerbate existing problems.

On this basis a contribution towards the Slough Transport Strategy is required so that the implementation of schemes within the Strategy to better promote access into, and management of parking and congestion in the Poyle area of the Borough can be brought forward. A contribution of £20,500 is considered commensurate with the additional traffic likely to be generated by this development and is consistent with the approach taken on other developments within the Borough.

The applicant will need to enter into a section 106 agreement with Slough Borough Council, this s106 agreement will obligate the developer to enter into a S278 Agreement

or Minor Works Agreement depending on scale of access improvements for the satisfactory implementation of the works identified in the highways schedule and for the collection of the contributions schedule. It is not clear whether the developer seeks to stop up any sections of the public highway to implement this development.

The contributions schedule includes:

- £3,000 Implementation of Traffic Regulation Orders (prior to commencement)
- £2,500 Landscaping Improvement (prior to occupation)
- £15,000 Transport contribution (prior to occupation)

The highways schedule includes:

- Installation of crossover / junction
- Reconstruct the footway fronting the application site
- Reinstatement of redundant access points to standard to footway construction
- Installation of street lighting modifications (as necessary)
- Drainage connections
- Dedication as highway maintainable at the public expense, free of charge, of sight line areas.

Recommendation

Subject to the securing of the S106 contribution for £20,500 together with the alterations to the drawings requested and the submission of tracking I would not raise a highway objection.

Conditions

1. No part of the development shall be occupied until the new means of access has been sited and laid out in accordance with the details to be subsequently submitted and approved in writing by the Local Planning Authority and constructed in accordance with Slough Borough Council's Design Guide.

Reason: In order to minimise danger, obstruction and inconvenience to users of the highway and of the development.

2. No part of the development shall be occupied until the redundant means of access has been removed and the footway reinstated in accordance with the approved drawings and constructed in accordance with Slough Borough Council's Design Guide.

Reason: In order to minimise danger, obstruction and inconvenience to users of the highway and of the development.

3. No part of the development shall begin until visibility splays have been provided on both sides of the access between a point 2.4 metres along the centre line of the access measured from the edge of the carriageway and a point 25 metres along the edge of the carriageway measured from the intersection of the centre line of the access. The area contained within the splays shall be kept free of any obstruction exceeding 0.6 metres in height above the nearside channel level of the carriageway.

Reason: To provide adequate intervisibility between the access and the existing public highway for the safety and convenience of users of the highway and of the access.

4. No part of the development shall commence until details showing the provision of a secure cycle store and an unobstructed footway link to accord with the Local Planning Authority's "Cycle Parking Standards" has been submitted to and approved in writing by the Local Planning Authority. No part of the development shall then be occupied until the cycle store and footway link have been laid out and constructed in accordance with the approved details and that area shall not thereafter be used for any other purpose.

Reason: To ensure that adequate and convenient cycle storage is provided to accord with Local Plan standards.

5. The development shall not begin until details of the disposal of surface water from the highway have been approved in writing by the Local Planning Authority and no dwelling shall be occupied until the works for the disposal of surface water have been constructed in accordance with the approved details.

Reason: To minimise danger and inconvenience to highway users.

6. The gates shall be secured in an open position during any hours in which the development is in operation.

Reason: In order to minimise danger, obstruction and inconvenience to users of the highway and of the development.

Informative(s) required

Should the application be revised in accordance with my comments the following informative(s) will apply.

The applicant is advised that if it is intended to use soakaways as the method of dealing with the disposal of surface water then the permission of the Environment Agency will be necessary.

The permission hereby granted shall not be construed as authority to obstruct the public highway by the erection of scaffolding, hoarding, skip or any other device or apparatus for which a licence must be sought from the Highway Authority.

The applicant will need to take the appropriate protective measures to ensure the highway and statutory undertakers apparatus are not damaged during the construction of the new unit/s.

Prior to commencing works the applicant will need to enter into a Section 278 Agreement of the Highways Act 1980 / Minor Highway Works Agreement with Slough Borough Council for the implementation of the works in the highway works schedule. The applicant should be made aware that commuted sums will be payable under this agreement for any requirements that burden the highway authority with additional future maintenance costs.

The above conditions will be attached to any final permission. Negotiations have commenced with the owners of the site with regards to the Section 106 payments and

The following comments have been received from the Council's Drainage Engineer:

The FRA it's well written but unfortunately some of the information they have received from other authorities is not correct however this does not change the assessment. The proposed surface water disposal is by infiltration using a crated system it has been designed on assumed ground conditions (I would suggest slightly optimistic) so needs to be confirmed by condition, following a ground investigation, in any consent. The site is not land locked so if the design proves inadequate for the site there is potential for exceedance flow to be accommodated.

The following condition would therefore be attached to any final permission:

The development shall not begin until details of on and off site drainage works have been submitted to and approved in writing by The Local Planning Authority. No works which result in the discharge of ground or surface water from the site shall be commenced until

the off-site drainage works detailed in the approved scheme have been completed

REASON to ensure that foul and water discharge from the site is satisfactory and shall not prejudice the existing sewerage systems in accordance with Policy EN31 of The Local Plan for Slough 2004.

As set out in paragraph 9.2 of the officers report there is a mature tree screen on the boundary of the site with Poyle Road. It is noted that some trees in the site will be felled as a result of this proposal and it is therefore considered appropriate to request an arboricultural report therefore be prepared and a condition be added to any permission to ensure that works are undertaken in accordance with the report.

The applicants have been requested to show a drivers welfare area within the building and this will also be secured via condition should approval be granted.

Paragraph 2.1 of the officers report mention inclusion of a mezzanine floor but this is incorrect and should be deleted.

Paragraph 11.3 of the officers report stated that the proposed building will be smaller than the existing buildings which is incorrect and the highway and transport comments above should be noted with regards to this element of the report.

CHANGE OF RECOMMENDATION TO:

Delegate the planning application to Strategic Lead Planning Policy for the completion of a Section 106 Agreement consideration of a tree report, finalising conditions and final determination.

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SLOUGH BOROUGH COUNCIL**REPORT TO: PLANNING COMMITTEE****DATE: 1st June 2015****PART 1****FOR INFORMATION****Planning Appeal Decisions**

Set out below are summaries of the appeal decisions received recently from the Planning Inspectorate on appeals against the Council's decisions. Copies of the full decision letters are available from the Members Support Section on request. These decisions are also monitored in the Quarterly Performance Report and Annual Review.

| WARD(S) | ALL | |
|-----------------------|--|---|
| Ref | Appeal | <u>Decision</u> |
| 2013/00102/ENF | 49, Bradley Road, Slough, SL1 3PL Without planning permission, the erection of a roof canopy at the rear of the Land forming an extension linking the rear of the dwelling house to the single storey outbuilding located in the rear garden. | Appeal Dismissed 23rd April 2015 |

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MEMBERS' ATTENDANCE RECORD 2014/15
PLANNING COMMITTEE

| COUNCILLOR | 19/06/14 | 24/07/14 | 03/09/14 | 16/10/14 | 27/11/14 | 08/01/15 | 17/02/15 | 01/04/15 | 29/04/15 |
|--------------|----------|----------|----------|----------|----------|----------|----------|----------|----------|
| Ajaib | P | P | P | P | P | P | P | P | P* |
| Bains | P | P | P | P | Ap | P | P | P | P |
| Dar | P | P | P | P | P | P | P | P | P |
| M. Holledge | P | P | P | P | P | P | P | P | P |
| Plenty | P | P | P | P | P | P | P | P | P |
| Rasib | P | P | P | P | Ap | P | Ap | P | Ap |
| Sidhu | P | P* | P | P | Ap | P | P* | A | P |
| Smith | P | P | P | P | P | P* | Ap | A | P* |
| Swindlehurst | P | P* | Ap | P* | P | P | P | Ap | P* |

P = Present for whole meeting
Ap = Apologies given

P* = Present for part of meeting
Ab = Absent, no apologies given

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